

Town of Sweden, Maine Fire Station Conditional Use Permit Application
 Package
 11-22-2023

of Pages

1. SW-FSBC Conditional Use Permit (CUP) Application Letter 2023-10-10	12
2. SW-FSBC 2023-10-10 Conditional Use Application submittal	4
3. SW-FSBC Architectural Drawings	7
4. Exhibit A Excerpt from The Town of Sweden, Maine Comprehensive Plan approved in 2004.	1
5. Exhibit B. SW-TMM 2006-03-18 See Article 17	16
6. Exhibit C. Plan of Land owned by the Town of Sweden, Maine	1
7. Exhibit D. SW-TMM 2021-03-27 See Article 44	15
8. Exhibit E. SW-FSBC Ltr from SB (email) dated 2023-09-24 asking FSBC to submit CUP Application	1
9. Exhibit F. SW-FSBC Bid Plans for proposed station 2023-11-21	6
10. Exhibit G. Ltr to FSBC dated 9-11-2023 from Engr re Soil Survey + Med Intensity Soil Survey	2
11. Exhibit H. SW-FSBC Ltr to FSBC dated 2022-01-06 from Longview Partners Septic Site Evaluation	3
12. Exhibit I. SW-FSBC Driveway Entrance Permit approved 2022-11-17	6
13. Exhibit J. SW-FSBC 2023-10-02 Meeting Record	2
14. Exhibit K. SW-FSBC 2023-10-10 Proposed Parking Lot Layout	1
15. Exhibit L. SW-FSBC Construction Permit from the State of ME Fire Marshall Extended 2023-08-15	1

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

Planning Board, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040

October 10, 2023

Re: Conditional Use Permit for Town of Sweden, Maine proposed fire station

Dear Planning Board,

At the request of the Select Board, the Town of Sweden Fire Station Building Committee (FSBC) is making application on behalf of the Town. The Conditional Use Application is for Municipal Use in a Residential Zone as allowed by Section VIII. D. of the *Town of Sweden, Maine, Zoning and Land Use Ordinance Seventh Revision March 21, 2009*.

Findings of Fact:

In the Town of Sweden, Maine Comprehensive Plan approved in 2004; Section II.J. Public Facilities and Services; one of the implementation strategies calls for the Town to “Commence detailed planning, make grant applications, and appropriate funds for the construction, in the designated growth area, new OSHA-approved fire station, and maintain the standards required for continual mutual aid with neighboring communities.”

See Exhibit A: Excerpt from The Town of Sweden, Maine Comprehensive Plan approved in 2004.

At the Town Meeting on March 18, 2006, voters approved appropriation of \$16,500 from surplus as follows: Article 17: “To see what sum the Town will authorize to take from surplus for the purpose of Purchasing Land for the Construction of a new Fire Station and allow the balance to carry over. The land was purchased from Ellen Valley in 2007.

See Exhibit B: Sweden Town Meeting Minutes from March 18, 2006

See Exhibit C: Plan of Land Owned by the Town of Sweden dated 8-31-2006.

Map R05 Lot 43-A includes the piece of land donated by the Sweden Community Church for the town office parking lot, the piece of land purchased from Roland Valley for the new Town office, and the piece of land purchased from Ellen Valley for construction of a new Fire Station.

At the Town Meeting on March 27, 2021, voters approved Article 44: “To see if the Town will vote to take from surplus up to \$32,000 for the purpose of developing the design and bid documents for a proposed Fire Station. Phase I cost Approximately \$8,000 for a conceptual design and a draft budget. Phase II Approximately \$26,000 for final design and bid document. (Some funding is available from a carry forward account).”

The SB signed the contract in April of 2022 and the plans were substantially complete in January of 2023. Bidding was delayed so an application could be made to the USDA for funding (USDA funding has different bidding requirements). The project is set to go out to bid in October of 2023.

See Exhibit D: Sweden Town Meeting Minutes from March 27, 2021

The Select Board (SB) has appointed the FSBC as the agent for the Town in making the application for the Conditional Use Permit (CUP) for a municipal use in a residential zone.

See Exhibit E: Ltr. to FSBC (email) dated September 24, 2023, from the SB requesting assistance.

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

A Conditional Use Application was submitted to the Planning Board by email on October 10, 2023, in accordance with the Town of Sweden Zoning and Land Use Ordinance, 2009.

Conclusions of Law:

Sweden Zoning and Land Use Ordinance, 2009

XIII. CONDITIONAL USE

The proposed use of land or buildings that is permitted only after review and approval by the Planning Board is called a CONDITIONAL USE. (See Section VIII, Zone Requirements, Section IX, Non-Conformance, and Section XIV, Definitions)

A. Establishment of Conditional Uses

1. The Planning Board is hereby authorized to hear and decide upon applications for Conditional Use Permits in accordance with State law and the provisions of this Ordinance.
2. **An application for a Conditional Use is required when** the Code Enforcement Officer determines, after inspection, that **the proposed use is not a permitted use but may be allowed as a Conditional Use as outlined in Section VIII** and Section IX of this Ordinance. The Code Enforcement Officer must notify the Planning Board in writing after making this determination.
3. The Planning Board shall hear and approve, approve with conditions and/or modifications, or disapprove all applications for Conditional Use Permits.
4. No Conditional Use Permit shall be authorized unless specific provisions for such Conditional Use are made in this Ordinance.

B. Application for Conditional Use

1. **A person, or his agent**, informed by the Code Enforcement Officer that he requires a Conditional Use Permit **shall apply to the Planning Board in writing for the permit**. No Conditional Use shall be commenced until the property owner of record, or his agent (who must be legally designated by the property owner), shall have received the required permit.
 - ***The SB has appointed the FSBC as the agent for the Town in making the application for the Conditional use Permit.***
See Exhibit E: Ltr to FSBC (email) dated September 24, 2023, from the SB requesting assistance.
2. The fee for Conditional Use Permits will be 100% of the costs incurred by the Planning Board in considering the Application, but in no case less than as established by the Municipal Officers in the Town of Sweden Fee Schedule, which minimum fee must be submitted with the application. This fee is intended to cover the cost to the town of the administrative processing of the application, including notification, advertising, mailings, and similar costs. In addition to the application fee, the applicant may also be required to pay a technical review fee to defray the town's legal and technical costs of the application review. The technical review fee may be used by the Planning Board to pay reasonable costs incurred by the Board, at its discretion, which relate directly to the review of the application pursuant to the review criteria. Such services may

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

include, but need not be limited to, consulting, engineering or other professional fees, and attorney fees.

Due to the unique nature of the application (made on behalf of the Town). The FSBC requests that payment of the fee be waived. Expenses related to the application should be documented and ultimately posted to the FSBC account by journal entry.

3. In order to secure the information upon which to base its decision the Planning Board **may require** the applicant to furnish the following in addition to any other information required for a complete Conditional Use Permit application:

The following information is included in the application package:

- a. Plan of the area showing contours at intervals determined by the Planning Board and reference to Mean Sea-Level, high-water elevation, ground water conditions, bedrock slope and vegetative cover. ***See Exhibit F. Bid Package Drawing C.2***
- b. High intensity soil survey with the soils information superimposed on the plot plan in accord with the USDA Soil Conservation Service National Cooperative Soil Classification. This survey is to be done by a State approved Soil Scientist.
See Exhibit G Statement from Ross Cudlitz, PE OSWCD District Engineer below. (A medium Intensity Soil Survey of the area in lieu of the High Intensity Soil Survey has been included for information purposes).

“Planning Boards, as well as DEP, have the prerogative to request a HISS (High Intensity Soil Survey) in lieu of the readily available USDA MISS (Medium Intensity Soil Survey) when they believe the soils on the site are not representative of the MISS for the intended use. Examples of when HISS is desired are dense subdivisions with septic systems, projects with stormwater infiltration systems, land with wetlands where developable area is marginal and lands with high seasonal water tables. The Skerry soils (HSG “B”) represented in the MISS from the USDA Web Soil Survey (WSS) are adequate and sufficient for the intended development. The Site Evaluator has already provided a site soil investigation for the septic system in the only area of the proposed development where definitive soils identification is necessary. As a project reviewer for municipalities, I am more than comfortable with the degree of investigation provided by the USDA WSS as it pertains to this small site expansion.”

- c. Location of existing and proposed buildings, parking areas, traffic access, driveways, piers, open spaces and landscaping; ***(See Exhibit F. Bid Package Drawing C.1)***
 - d. Plans of buildings, sewerage disposal facilities and water systems; ***(See Exhibit H. Letter to FSBC dated 1-6-2022 from Longview Partners, LLC including soils report, soil test pit results and potential wastewater disposal plan.***
 - e. Any other pertinent information necessary to determine if the proposed use meets the provisions of this Ordinance.
4. The permit is automatically terminated with any change in use of the property, change in ownership of the property, or change in occupancy of the property.

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

C. Hearing Procedure Pertaining to Conditional Use Requests

1. The Secretary of the Planning Board shall schedule a Public Hearing within 45 days following the Board's receipt of a complete Conditional Use Permit application.
2. The Secretary of the Board shall, on receipt of any such application, forthwith, send or hand deliver notice of the time and place of the Public Hearing and the nature of the Conditional Use request, to the Selectmen of the Town of Sweden, the Code Enforcement Officer and the Chairman of the Sweden Board of Appeals.
3. The Secretary of the Board, shall, at least 20 days prior to the Public Hearing, send (certified mail, return receipt requested) notice of the time and place of the Public Hearing and the nature of the Conditional Use request, to the applicant, and to all abutting property owners. For the purpose of determining ownership of the subject and/or abutting properties, the records of the Town Assessors shall be conclusive.
4. The Secretary of the Board shall, at least 10 days prior to the Public Hearing, place a notice of Conditional Use Request in the newspaper of general circulation in the Town of Bridgton stating the nature of the Conditional Use Request, the location of any property involved and the time and place of the Public Hearing.
5. The Chairman of the Planning Board shall preside at all such meetings and shall serve as official spokesman of the Board. The Secretary shall serve as Vice-Chairman of the Board.
6. At any hearing the applicant may appear by agent or attorney. Hearings shall not be continued to other times except for good cause.
7. The Sweden Code Enforcement Officer, as well as designated technical and/or professional advisors to the Planning Board, may appear at all hearings and may present plans, photographs or other material that they deem appropriate to an understanding of the application. Members of relevant committees (e.g., the Historical Society, Conservation Committee) shall be encouraged to attend hearings and comment as appropriate.
8. The applicant's side of the case shall be heard first. To maintain orderly procedure each side shall proceed without interruption. Questions may be asked through the Chairman. All persons at the hearing shall abide by the order and direction of the Chairman.
9. The Planning Board may receive any oral or documentary evidence but shall provide, as a matter of policy, for the exclusion of irrelevant, immaterial or unduly repetitious evidence. Every party shall have the right to present its case or defense by oral or documentary evidence, to submit rebuttal evidence, and to conduct such cross-examination as may be required for a full and true disclosure of the facts.
10. The Planning Board may provide by rule, which shall be recorded by the Secretary, for any matter relating to the conduct of any hearing, provided that any rule may be waived by the Chairman upon good cause shown.

D. Administrative Procedures Pertaining to Conditional Use Applications

1. The Secretary of the Planning Board shall:
 - a. Maintain a permanent record of all such Board meetings and of each individual vote. The final disposition of the Conditional Use Requests shall be recorded by resolution indicating the reason(s) for such final decision.

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

- b. Maintain a file containing all relevant correspondence.
 - c. Be responsible for maintaining those records that are required as part of the Conditional Use Request proceedings. The transcript of the testimony and/or tape recording of the proceedings, if any, and exhibits, together with all papers and requests filed in the proceedings, shall constitute the record. All decisions shall become part of the record and shall include a statement of finding and conclusions as well as the reason(s) or basis therefore, upon all material issues of fact, law or discretion presented and the appropriate order, relief or denial thereof.
2. All records to be maintained or prepared by the Secretary are deemed public, shall be filed in the Municipal Office, and may be inspected at reasonable times.
 3. A quorum of the Board necessary to conduct an official Board meeting for the purpose of considering a Conditional Use Request shall consist of at least three regular and/or alternate members.
- E. Factors Applicable to Conditional Uses.** It shall be the responsibility of the applicant to demonstrate that the proposed use meets all of the following criteria. The Board shall approve the application unless it makes written findings that one or more of these criteria have not been met.
1. The use will not have an adverse impact on the spawning grounds, fish, aquatic life, bird or other wildlife habitat.
This is a Municipal Use and is not in a shoreland zone so it will not have an adverse impact on the spawning grounds, fish, aquatic life, bird or other wildlife habitat.
 2. The use will conserve shore cover and visual, as well as actual, access to water bodies.
The use meets these criteria because it is not near a water body.
 3. The use is consistent with the Comprehensive Plan. ***The use is as directed by the Town of Sweden, Maine Comprehensive Plan adopted in May of 2004 See Section II. J. Public Facilities and Services Implementation Strategy # 5 (Page 70) which states: Continue detailed planning, grant applications, and appropriate funding for the construction of a new fire station, and maintain the standards required for continual mutual aid with neighboring communities.***
 4. The need for a particular location for the proposed use. ***The proposed use is an allowed use and is appropriately located in a residential zone, adjacent to existing municipal uses.***
 5. Traffic access to the site meets the standards contained in this Ordinance, and traffic congestion has been minimized in accordance with performance standards in this Ordinance. ***A permit has been approved by the Maine Department of Transportation (MDOT) – See Exhibit I State of Maine Department of Transportation Entrance/Driveway permit issued 11-17-2022 as required for entrance on a State Aid Road (Rte. 93)***
 6. The site design is in conformance with all municipal flood hazard protection regulations. ***The site is not in a municipal flood zone.***
 7. Adequate provision for the disposal of all wastewater and solid waste has been made. ***A site evaluation has been included for disposal of all wastewaters (See Exhibit H. Letter to***

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

FSBC dated 1-6-2022 from Longview Partners, LLC including soils report, soil test pit and potential wastewater disposal plan.

8. Adequate provision for the transportation, storage and disposal of any hazardous materials has been made. ***There should be no hazardous materials as confirmed by the Fire Chief and the Assistant Fire Chief at a meeting on October 2, 2023. See Exhibit J. SW-FSBC 2023-10-02 Meeting Record.***
9. A storm water drainage system capable of handling a 25-year storm without adverse impact on adjacent properties has been designed and will be put into effect and maintained. ***See Exhibit F. Bid Package Drawings C.2. and C.3***
10. Adequate provisions to control soil erosion and sedimentation have been made. ***See Exhibit F. Drawing C.3. Erosion and Sediment Control***
11. There is adequate water supply to meet the demands of the proposed use, and for fire protection purposes. ***A new well will be drilled specifically for the fire station, providing a separate water supply.***
12. The provisions for buffer strips and on-site landscaping provide adequate protection to neighboring properties from detrimental features of the development such as, but not limited to, noise, glare, fumes, dust, odor and the like. ***See Exhibit F. Drawings C.1. and C.2. A 34' natural buffer and a stand of trees will be maintained from the structure to the abutting property. A 50 ft. vegetated buffer will be maintained as required along Rte. 93.***
13. All performance standards in this Ordinance applicable to the proposed use will be met. ***(See review of Section X. Performance Standards below)***
14. The proposed use will be compatible with all uses permitted in the underlying Zone and on abutting properties. ***(See review of Section VII Zone Requirements below)***
15. The proposed use will not have unreasonable impact upon Municipal facilities. ***The proposed use will have a positive impact on Municipal facilities by providing a service desired by voters for the health and safety of residents and taxpayers.***

F. Conditions Attached to Conditional Use Permits. Upon consideration of the factors listed above, the Planning Board may attach such conditions, in addition to those required elsewhere in this Ordinance, that it finds necessary to further the purpose of this Ordinance. Violation of any of these conditions is a violation of this Ordinance. Such conditions may include, but are not limited to:

- Periodic review of use
- Duration/limitation of use
- Specifications for types of vegetation
- Increased setbacks and yards
- Specified sewerage disposal and water supply facilities
- Period of operation
- Operational controls

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

- o Sureties
- o Deed restrictive covenants
- o Location of piers, docks, parking
- o Construction specifications, size/height limitations
- o Any other conditions necessary to fulfill the purpose of this Ordinance

G. Decisions.

1. The Planning Board shall render a decision within 30 days after a Public Hearing. Notice of any decision shall be mailed or hand delivered to the applicant or his agent, the Selectmen of the Town of Sweden, the Code Enforcement Officer, and the Chairman of the Board of Appeals within seven days of such a decision.
2. Upon notification by the Planning Board of the final disposition of a Conditional Use Request, the Code Enforcement Officer shall issue, issue with conditions and/or modifications, or deny such Permit.
3. A Conditional Use Permit secured under the provisions of this Ordinance by vote of the Planning Board shall expire if the work, change, or use is not commenced within one year of the date on which the Conditional Use is authorized, or if the work is not substantially completed within two years of said date.

H. Appeals. An appeal may be taken within 30 days after any decision rendered, by any party to Superior Court, from any order, approval or denial in accordance with Maine Rules of Civil Procedure. The hearing before Superior Court shall be without a jury.

Review of Section X. Performance Standards: The following Performance standards do not apply to this project:

X.	PERFORMANCE STANDARDS	19
A.	Agriculture	19
B.	Cluster Developments	19
C.	Elevation of Buildings in Flood Hazard Areas	20
E.	Historical Sites and Structures	21
F.	Home Occupations	22
G.	Manufactured Housing	23
H.	Mineral Exploration and Extraction	24
I.	Multi-Family Dwellings.....	25
J.	Nuclear Power Facilities.....	26
L.	Piers, Docks and Other Shoreland Construction	26
M.	Public Nuisances	27
N.	Public Utilities	28
O.	Recreational Vehicles.....	28
R.	Shorefront Common Areas.....	31
V.	Temporary Commercial Structures	33
W.	Timber Harvesting	33
X.	Vegetative Clearing within Shoreland Areas.....	35
Y.	Water Quality Protection	36
Z.	Wireless Communication Facilities	37

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

The following Performance Standards will be met as follows:

D. Erosion and Sedimentation Control:

1. All activities that involve filling, grading, excavation or other similar activities which result in un-stabilized soil conditions, and which require a permit shall require a written soil erosion and sedimentation control plan. The plan shall be submitted to the permitting authority for approval and shall include, where applicable, provisions for:
 - a. Mulching and revegetation of disturbed soil.
 - b. Temporary runoff control features such as hay bales, silt fencing or diversion ditches.
 - c. Permanent stabilization structures such as retaining walls or riprap.
2. In order to create the least potential for erosion, development shall be designed to fit with the topography and soils of the site. Areas of steep slopes where high cuts and fills may be required shall be avoided wherever possible and natural contours shall be followed as closely as possible. The amount of exposed soil at every phase of construction shall be minimized to reduce the potential for erosion.

Erosion and sedimentation control measures shall apply to all aspects of the proposed project involving land disturbance and shall be in operation during all stages of the activity.

3. Any exposed ground area shall be temporarily or permanently stabilized within one week from the time it was last actively worked, by use of riprap, sod, seed, and mulch or other effective measures. In all cases permanent stabilization shall occur within 9 months of the initial date of exposure. In addition:
 - a. Where mulch is used, it shall be applied at a rate of at least one bale per 500 square feet and shall be maintained until a catch of vegetation is established.
 - b. Anchoring the mulch with netting, peg and twine or other suitable method may be required to maintain the mulch cover.
 - c. Additional measures shall be taken where necessary in order to minimize siltation into the water. Such measures may include the use of staked hay bales and/or silt fences.
5. The Code Enforcement Officer is empowered to inspect and identify any existing source of erosion or sedimentation that has the potential to deliver soils or sediment to any water body protected by this Ordinance. Any such sources shall be immediately stabilized to temporarily prevent such erosion and sedimentation. Following temporary stabilization, which must be accomplished within one week of notice of violation, an erosion and sedimentation control plan and maintenance provisions shall be submitted for review within thirty days of notice of violation. The erosion stabilization plan shall demonstrate that the requirements of this Ordinance will be met. The erosion stabilization plan shall be completed within two years of notice of violation.

See Exhibit F Bid Package Drawing C.3. Erosion and Sediment Control

- K. Off-Road Parking.** Parking areas shall meet the shoreline setback requirements for structures. Off-road parking space for each dwelling unit shall be provided in the amount of 300 square feet. Public parking space for permitted businesses shall be one space for each employee, and one space for each three people expected to be served. Such space may be accomplished by garage space, driveway space or parking lot.

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

1. **Loading - Unloading.** Where permitted or allowed, commercial uses shall provide, as necessary, off-road loading/unloading facilities located entirely on the same lot as the building or use to be served so that trucks, trailers and containers shall not be located for loading, unloading or storage upon any public roadway.
2. **Landscape Buffering.** Required parking and loading spaces for nonresidential uses, where not enclosed within a building, shall be effectively screened from view by a continuous landscaped area not less than eight feet in width containing evergreen shrubs, trees, fences, walls or any combination thereof forming a visual barrier not less than six feet in height along exterior lot lines adjoining all residential properties, except that driveways shall be kept open to provide visibility for vehicles entering and leaving.

Parking will be provided in the expanded Town office parking lot which is shared by the Sweden Community Church and the Sweden Historical Society. Landscape buffering along the exterior lot line adjoining the residential property adjacent to the proposed fire station will be maintained. See Exhibit K Proposed parking lot layout

- P. Roads and Driveways:** *A permit has been approved by the Maine Department of Transportation (MDOT) See Exhibit I State of Maine Department of Transportation Entrance/Driveway permit issued 11-17-2022. as required for entrance on a State Aid Road (Rte. 93). The FSBC will request an additional permit be issued by the Code Enforcement Officer along with a building permit.*

- Q. Sanitary Provisions** To promote health, safety and general welfare, and to protect groundwaters and public and private water supplies from contamination or nutrient enrichments, all solid waste and subsurface sewage disposal systems shall be installed in conformance with the latest revision of the State of Maine Subsurface Wastewater Disposal Rules. All hazardous waste, industrial and septic sludge shall be handled and disposed of in an appropriate manner in accordance with current state regulations. ***(See Exhibit H. Letter to FSBC dated 1-6-2022 from Longview Partners, LLC including soils report, soil test pit and potential wastewater disposal plan. A site evaluation has been included for disposal of all wastewater.***

S. Signs No sign shall:

- a. Exceed 16 square feet in area; or in the Natural resource Protection Zone, Limited Residential Zone and Stream Protection District exceed six square feet in area.
- b. Have any visible moving or flashing parts. (unless required for safety).
- c. Be located within five feet of a roadway line or any abutter's lot line.
- d. Be placed in such a position as to endanger traffic or obscure the view.
- e. Be erected or placed in the Town of Sweden that advertises a business not located in the Town of Sweden.
- f. Extend higher than 20 feet above the ground.

Signage will be kept to a minimum. It will be limited to identifying signage on the building and any signage required by State Law to ensure the safety of those travelling along the adjacent roadway.

- T. Soils** All land uses shall be located on soils in or upon which the proposed uses or structures can be established or maintained without causing adverse environmental impacts, including severe erosion, mass soil movement, improper drainage, and water pollution, whether during or after construction. Proposed uses requiring subsurface waste disposal shall require a soils report based on

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

an on-site investigation and prepared by state-certified site evaluators. The report shall be based upon an analysis of the characteristics of the soil and surrounding land and water areas, maximum ground water elevation, presence of ledge, drainage conditions, and other pertinent data that the evaluator deems appropriate. The soils report shall include recommendations for a proposed use to counteract soil limitations where they exist.

See Exhibit G Statement from Ross Cudlitz, PE OSWCD District Engineer and A medium Intensity Soil Survey of the area in lieu of the High Intensity Soil Survey has been (included for information purposes).

(See Exhibit H. Letter to FSBC dated 1-6-2022 from Longview Partners, LLC including soils report, soil test pit and potential wastewater disposal plan.

U. Storm Water Runoff.

1. All new construction and development shall be designed to minimize storm water runoff from the site in excess of the natural predevelopment conditions. Storm drainage management and construction shall meet all applicable standards. ***See Exhibit F. Bid Package Dwgs C.2, C.3 and C.4***
2. Where possible, existing natural runoff control features, such as berms, swales, terraces and wooded areas shall be retained in order to reduce runoff and encourage infiltration of storm waters.
3. Storm water runoff systems shall be maintained as necessary to ensure proper functioning, and any resultant deposited materials shall be properly disposed of.

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

Review of Section VIII. ZONE REQUIREMENTS

A. General Requirements. The following general requirements shall apply to all Zones:

1. **Development Permits and Requirements.**
 - a. A Building Permit and associated Plumbing Permit shall be required for all buildings and structures according to the provisions of this ordinance. A building permit will be obtained from the Code Enforcement Officer. **See Exhibit L. Construction permit from the Office of the State Fire Marshall dated 1-17-2023. The permit was renewed on 8-15-2023.**
 - b. Permit applicants are responsible for any reasonable cost to the town incurred in approving any permit application. These costs may include, but need not be limited to, administrative and technical review costs.
 - c. Permitted Uses and Conditional Uses shall conform to all other applicable provisions of this Ordinance.
 - d. **Phosphorus Control. Does not apply to this project.**
2. **Dwellings.** No more than one dwelling unit shall be placed/constructed on a lot unless each dwelling unit meets the minimum lot size and all setback requirements specified for the underlying zone, except as may be otherwise provided for in this Ordinance.
3. **Structure Height Limit.** No building or structure shall exceed 50 feet in height, 35 feet in a Shoreland Zone as defined, including any features of buildings or structures such as, but not limited to, towers, ventilators, cupolas, and spires. Household antennas and residential chimneys are excepted from this limit. **The Structure is 34' in height.**
4. **Dimensional Requirements.** Any lot created after the effective date of the current revision to this Ordinance shall meet the following requirements.
 - a. **Lot Size:** Any lot shall meet the square footage requirement specified for its underlying zone.
 - b. **Lot Frontage:** All lots shall front upon a road either publicly or privately maintained, and meet the minimum roadway frontage specified. If abutting a great pond, river, or a stream (as defined in the state's Shoreland Zone), lots shall meet the minimum shore frontage specified.
 - (1) Roadway frontage will be determined as horizontal distance between the intersections of the side lot lines and the front lot line.
 - (2) Shore lot frontage will be determined as the horizontal distance measured in a straight line between the intersections of the side lot lines with the shoreline at normal high-water elevation.
 - c. **Lot Depth-to-Shore-Frontage Ratios.** All lots created within a subdivision that has frontage on a great pond, river or stream (as defined in the state's Shoreland Zone), may not have a lot depth-to-shore-frontage ratio greater than five to one.
 - d. **Structure Setbacks.** No building or structure shall be erected or located on a lot, or recreational vehicle located on a lot, or campsite placed on a lot, unless it shall be setback from the roadway centerline, from the side and rear lot lines, and from the normal high-water line of any lake, pond, river or stream (as defined in the state's Shoreland Zone), the distance specified for structure setbacks in the respective Zone.
 - e. **Roadway Buffer Vegetation.** All lots, regardless of zone, shall be required to maintain a 50-foot buffer strip (see Definitions) along the roadway.
5. **Back Lots. Does not apply to this project**
6. **Water Crossings. Does not apply to this project**

**Fire Station Building Committee, Town of Sweden Maine
147 Bridgton Road
Sweden, Maine 04040**

7. **Lot Coverage.** The total area of all structures, parking and other non-vegetated surfaces, within a Shoreland Zone, as defined, shall not exceed 20% of the lot, or portion thereof, located within the Shoreland Zone.

Map R05 Lot 43-A is in the Residential Zone so the following applies:

- D. Residential Zone.** The Residential Zone includes all areas, not otherwise delineated as a Zone on the Sweden Zoning Map, suitable for residential development as well as recreational, agricultural, and home occupation uses.
1. **Purpose.** To provide a less restrictive area for residential development and other compatible land uses than the other designated Zones; and to preserve and maintain rural-residential character, as much as possible, along the major public roadways.
 2. **Permitted Uses.** The following uses will be permitted in the Residential Zone.
 - All permitted uses allowed in the Natural Resource Protection Zone and Limited Residential Zone
 - Yard, garage, barn and tag sales, if no more than four days per month
 3. **Conditional Uses.** The following uses may be allowed only upon approval of the Planning Board in accordance with the provisions of Section XIII. **CONDITIONAL USE:**
 - **Municipal use and public utilities** *The proposed Fire Station is a municipal use which is allowed as a conditional use,*
 4. **Prohibited Uses.** Uses not specifically allowed as Permitted or Conditional Uses are prohibited.
 5. **Dimensional Requirements:**
 - a. Minimum Lot Size: 70,000 square feet (approximately 1½ acre). **Map R05 Lot 43-A is 2.5 Acres**
 - b. Minimum Lot Frontage:
 - 150 feet roadway frontage road **Map R05 Lot 43-A has 170.14 LF of road frontage**
 - 200 feet shoreline frontage
 - c. Structure Setbacks:
 - 75 feet from roadway centerline **The proposed structure setback is approximately 169 ft from roadway centerline**
 - 100 feet (at all points) from high-water line of a great pond or river; 75 feet (at all points) from a stream or upland edge of a wetland
 - Height of structure from rear and side lot lines, but not less than 30 feet. **The structure is 34 ft in height, and the structure setback from the side lot line is 34 feet.**
 6. **Performance Standards.** In addition to any other requirements of this Ordinance, Permitted and Conditional Uses allowed shall also meet the requirements of all applicable land use and performance standards as contained in Section X. **PERFORMANCE STANDARDS. See review above.**

**BUILDING OR USE
PERMIT APPLICATION
TOWN OF SWEDEN
147 Bridgton Road
Sweden, ME 04040**

FOR OFFICE USE ONLY	
PERMIT NO.	_____
ISSUE DATE	_____
FEE AMOUNT	_____
APPROVED BY	_____

1. PROPERTY OWNER Town of Sweden, Maine	2. PHONE 207-647-3944	8. MAP R05	9. LOT 43-A	10. ZONE Residential
---	---------------------------------	----------------------	-----------------------	--------------------------------

3. PROPERTY ADDRESS 147 Bridgton Rd. Sweden, Maine 04040	11. LOT OF RECORD WHAT YEAR <u>2007</u>
--	---

4. OWNER'S ADDRESS Same	12. CONTIGUOUS LOTS LOT NO. (S) <u>N/A</u>
-----------------------------------	--

5. CONTRACTOR To be determined (TBD)	6. PHONE _____	13. HISTORICAL BUILDING WHAT YEAR <u>N/A</u>
--	--------------------------	--

7. CONTRACTOR'S ADDRESS _____	14. SPECIAL ZONES <u>N/A</u> <input type="checkbox"/> SHORELAND <input type="checkbox"/> NATURAL RESOURCE PROTECTION <input type="checkbox"/> FLOOD ZONE <input type="checkbox"/> AQUIFER PROTECTION
---	---

15. PROPOSED USE Municipal Fire Station	16. COST TBD
---	------------------------

17. PROJECT DESCRIPTION Conditional Use - Municipal Fire Station in a Residential Zone
--

18. NUMBER OF STORIES
PRESENT <u>0</u>
PROPOSED <u>1</u>

19. HEIGHT OF BUILDING:
PRESENT <u>0</u>
PROPOSED <u>34'</u>

20. NUMBER OF BATHROOMS
PRESENT <u>0</u>
PROPOSED <u>2</u>

21. NUMBER OF BEDROOMS
PRESENT <u>0</u>
PROPOSED <u>0</u>

22. PRESENT SEPTIC SYSTEM IS APPROVED FOR:
<u>N/A</u> BEDROOMS

23. YEAR ROUND USE <u>X</u>
SEASONAL USE _____

24. TYPE OF WATER SUPPLY

PRIVATE DUG WELL

COMMUNAL DRILLED WELL

25. TYPE OF SEWAGE DISPOSAL

PRIVATE

COMMUNAL

ADDITIONAL PERMITS, APPROVALS AND INSPECTIONS REQUIRED

PLUMBING, INTERIOR BOARD OF APPEALS FIRE CHIEF

SEPTIC / HHE200 D.E.P. ROAD OPENING Received

SEPTIC VARIANCE PERMIT BY RULE EROSION CONTROL PLAN

PLANNING BOARD OTHER _____

SHORELAND FIRE MARSHALL Received _____

_____ _____

PROPERTY INFORMATION

26. FRONTAGE

170.14 FT.

NON CONFORMING

27. MORE THAN ONE USE EXISTING ON THE PROPERTY. ACCESSORY USE Town Office

NON CONFORMING

28. SETBACKS

169' 34' 170'

ROAD SIDE REAR

_____ NORMAL HIGH WATER

29. NUMBER OF DWELLING UNITS PRESENTLY EXISTING ON THE LOT

0

30. LOT SIZE (IN SQ. FT. OR ACRES)

2.5 Acres

NON CONFORMING

31. TOTAL SQ. FT. OF ALL BUILDING:

PRESENT 1,783

PROPOSED 1,783 + 4,864 = 6,647 SF

32. LOT COVERAGE (IN PERCENT)

PRESENT 1.64%

PROPOSED 6.10%

ZONE % _____

33. NUMBER OF OFF STREET PARKING SPACES

PRESENT	<u>12</u>
PROPOSED	<u>18</u>
	COVERED UNCOVERED

34. BUILDING PERMITS DO NOT INCLUDE PLUMBING, SEPTIC OR ELECTRICAL WORK. BUILDING PERMITS ARE VALID FOR TWO YEARS. ANY FALSE INFORMATION MAY INVALIDATE A BUILDING PERMIT AND STOP ALL WORK.

Julio A McQueen 10-10-2023

APPLICANT OR AGENT DATE

PLOT PLAN

PLEASE INCLUDE ALL SETBACK DISTANCES FROM PROPERTY BOUNDRIES, ROADS, STREETS, AND RIGHT OF WAYS; ALL WETLANDS AND WATERBODIES; ANY EXISTING WELLS AND SEPTIC SYSTEMS. INCLUDE 100 FT. SHORELAND SETBACK OR FLOOD ELEVATIONS IF APPLICABLE. SHOW ALL PROPOSED DECKS, PORCHES AND OUTBUILDINGS.

See SW-FSBC Exhibit F. Drawings C.1 and C.2

ELEVATIONS with all dimensions

See Drawings A101 to A107

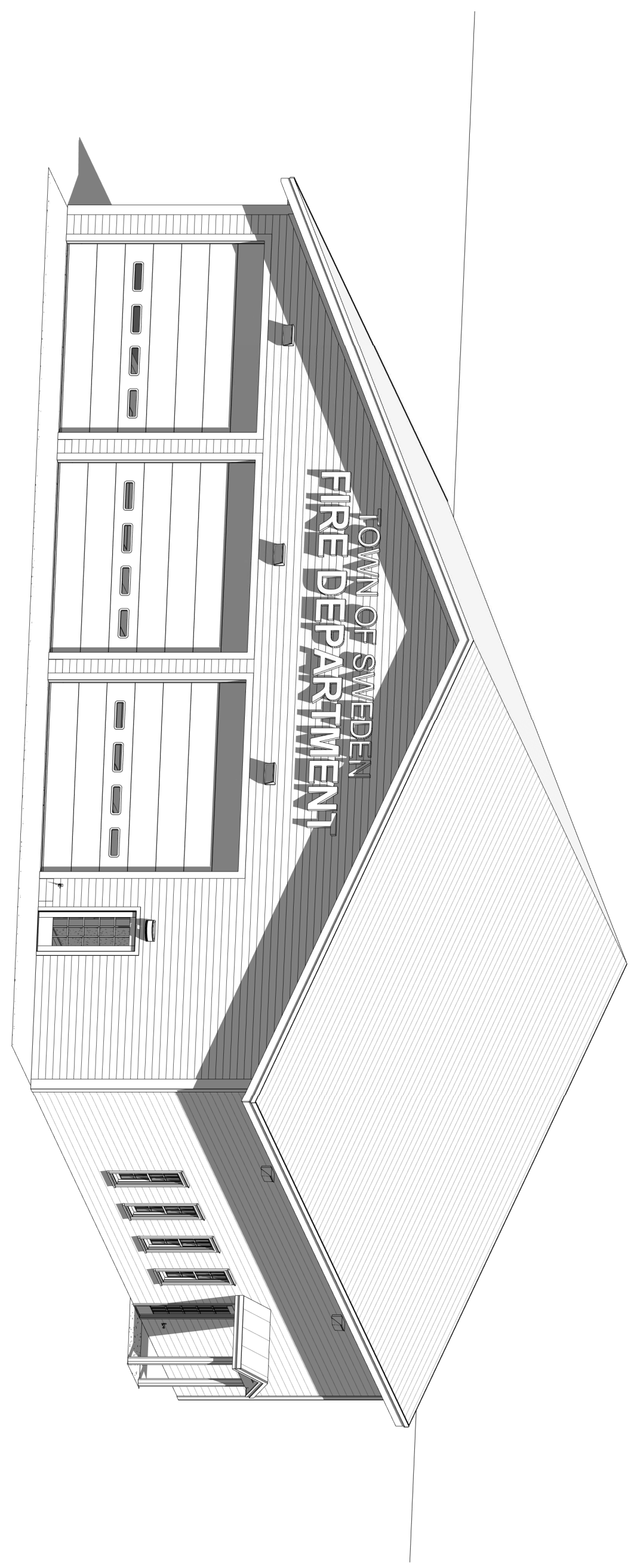
**A-101 dated 1-9-2023
A-102 dated 12-14-2022
A-103 dated 1-9-2023
A-104 dated 12-14-2022
A-105 dated 12-14-2022
A-106 dated 12-14-2022
A-107 dated 12-14-2022**

**FRONT OR REAR VIEW
with all dimensions**

**SIDE VIEW
with all dimensions**

PROPOSED FIRE STATION TOWN OF SWEDEN

OCTOBER, 2022
SWEDEN, MAINE



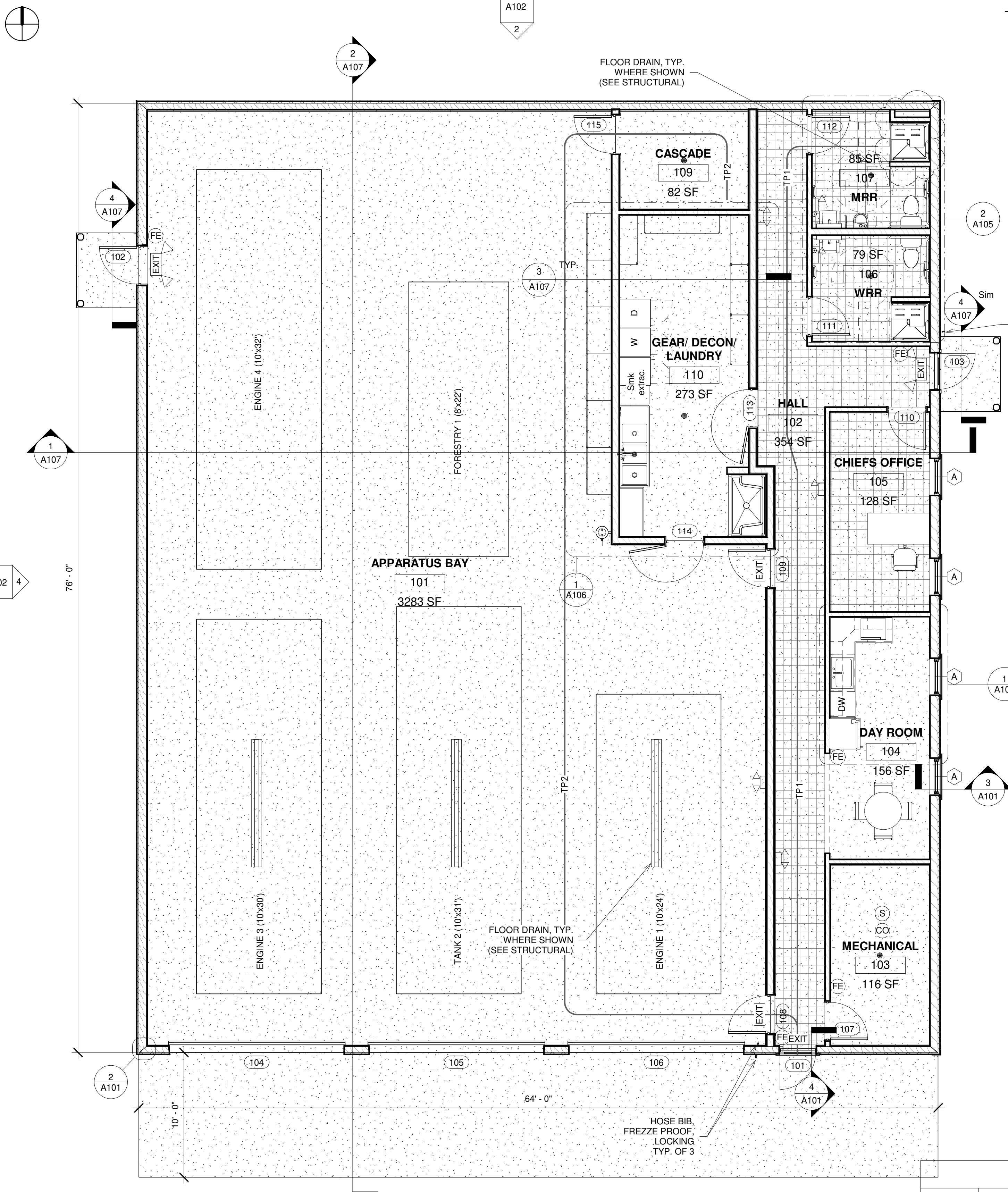
Plymouth Engineering, Inc.

8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

PROPOSED FIRE STATION TOWN OF SWEDEN

J:\2019\19080 - Sweden Fire Station - Sweden - Town of Sweden\J. Drawings\BUILDING\19080A.rvt

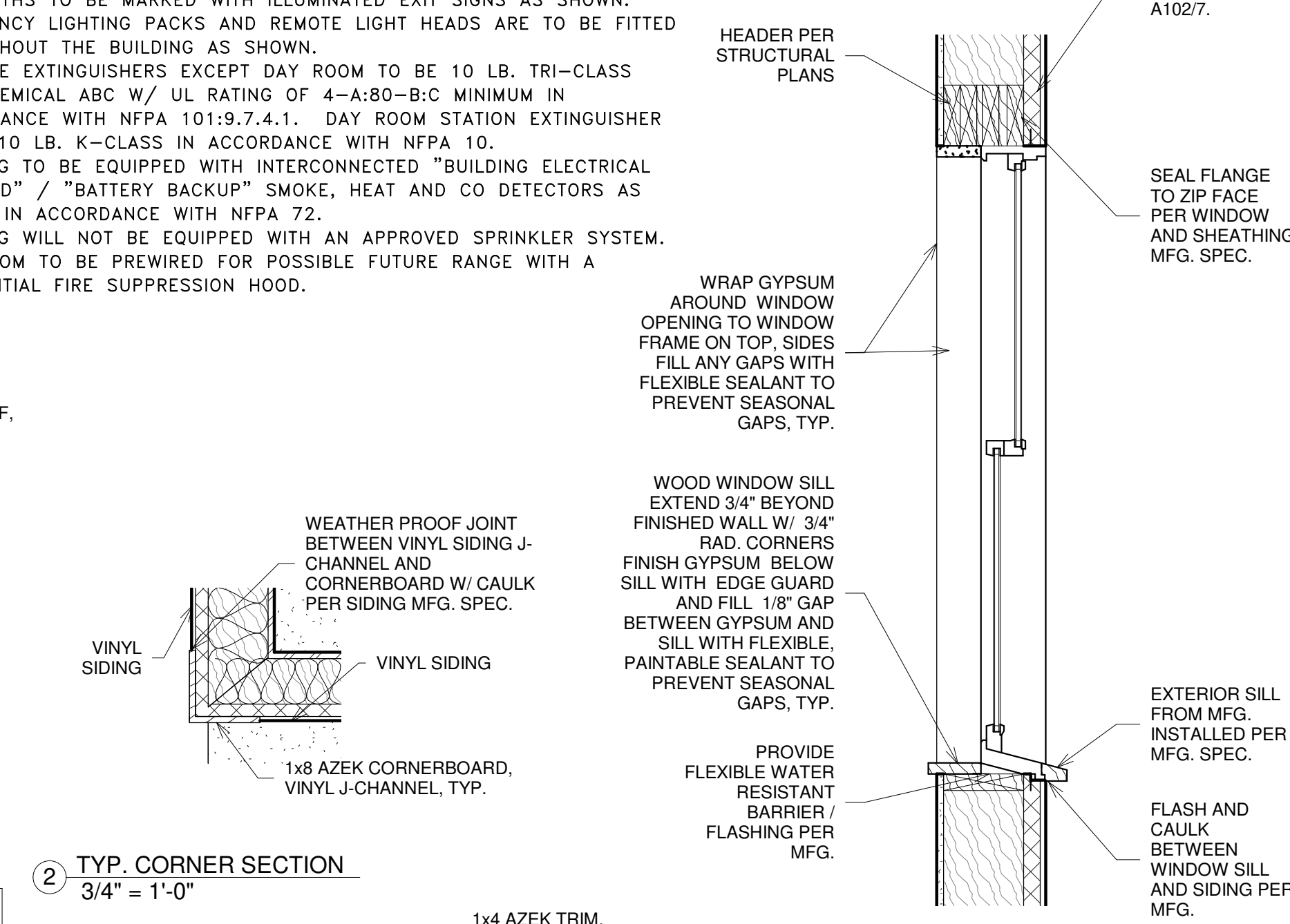
CIVIL	ARCHITECTURAL	STRUCTURAL	MECHANICAL	ISSUED FOR:
EX EXISTING CONDITIONS C1 CIVIL SITE PLAN C2 GRADING PLAN C3 EROSION AND SEDIMENT CONTROL DETAILS C4 CONSTRUCTION DETAILS	COVER AND SHEET INDEX A101 FLOOR PLAN A102 ELEVATIONS A103 FIRST FLOOR LAYOUT PLAN A104 REFLECTIVE CEILING PLAN A105 INTERIOR ELEVATIONS A106 INTERIOR ELEVATIONS, SCHEDULES A107 SECTIONS	S101 NOTES S102 FOUNDATION PLAN S103 DETAILS S104 FRAMING PLAN	M -	<input checked="" type="checkbox"/> PERMITTING <input checked="" type="checkbox"/> BID <input type="checkbox"/> CONSTRUCTION SET NO:



- NOTES:
1. INTENT OF THIS PLAN SET IS TO DEPICT A FLOOR PLAN AND ASSOCIATED ACCESSORIES REQUIRED TO MEET LIFE SAFETY, ADA, IBC AND MUBEC AS THEY RELATE TO OCCUPANCY, ACCESSIBILITY AND EGRESS REQUIREMENTS.
 2. BUILDING IS A NEW FIRE STATION CONTAINING APPARATUS, EMERGENCY GEAR STORAGE, MAINTENANCE AREAS, PERSONNEL SUPPORT FACILITIES (INCLUDING DAY AREA AND RESTROOMS) AND FIRE DEPARTMENT ADMINISTRATION OFFICES (CHIEFS OFFICE).
 3. EXIT PATHS TO BE MARKED WITH ILLUMINATED EXIT SIGNS AS SHOWN. EMERGENCY LIGHTING PACKS AND REMOTE LIGHT HEADS ARE TO BE FITTED THROUGHOUT THE BUILDING AS SHOWN.
 4. ALL FIRE EXTINGUISHERS EXCEPT DAY ROOM TO BE 10 LB. TRI-CLASS DRY CHEMICAL ABC W/ UL RATING OF 4-A-80-B-C MINIMUM IN ACCORDANCE WITH NFPA 101:9.7.4.1. DAY ROOM STATION EXTINGUISHER TO BE 10 LB. K-CLASS IN ACCORDANCE WITH NFPA 10.
 5. BUILDING TO BE EQUIPPED WITH INTERCONNECTED "BUILDING ELECTRICAL POWERED" / "BATTERY BACKUP" SMOKE, HEAT AND CO DETECTORS AS SHOWN IN ACCORDANCE WITH NFPA 72.
 6. BUILDING WILL NOT BE EQUIPPED WITH AN APPROVED SPRINKLER SYSTEM.
 7. DAY ROOM TO BE PREWIRED FOR POSSIBLE FUTURE RANGE WITH A RESIDENTIAL FIRE SUPPRESSION HOOD.

LEGEND

UNO, U.N.O.	UNLESS NOTED OTHERWISE
TBD, T.B.D	TO BE DETERMINED
CBO, C.B.O.	COLOR OR STYLE DETERMINED BY OWNER
DBO, D.B.O.	DESIGN OR DESIGNS BY OTHERS
HGT, H.G.T.	HEIGHT
TOW, T.O.W.	TOP OF WALL
AFF, A.F.F.	HEIGHT ABOVE FINISHED FLOOR
SF, S.F.	AREA IN SQUARE FOOT
AC, AC.	AREA IN ACRES
SRR, SUR, SR	SINGLE OCCUPANT RESTROOM OF ANY IDENTITY
MRR, MR, M.R.	MULTIPLE OCCUPANT IDENTIFYING AS MALE RESTROOM
WRR, FRR, W.R, F.R	MULTIPLE OCCUPANT IDENTIFYING AS FEMALE RESTROOM
WC, W.C.	WATER CLOSET
JC, J.C.	JANITORS CLOSET
MECH, MECH.	MECHANICAL ROOM
MU, MP, M.U, M.P.	MULTIPLE USE OR PURPOSE SPACE OR ROOM
ALT	ALTERNATE
TYP.	TYPICAL OR IN-KIND
O.C.	ON CENTER
OOF, OOC	OUTSIDE EDGE TO OUTSIDE EDGE OF FOUNDATION OR CONCRETE



FINISH SCHEDULE ABBREVIATION KEY

RC	- 4" RESILIENT COVE BASEBOARD
VB	- 4" VINYL COVE BASEBOARD
CB	- CERAMIC BASE TILE
AT	- 4" AZEK COMPOSITE TRIM, PAINTED
GWB	- GYPSUM WALL BOARD
EXP	- EXPOSED
MP	- METAL LINER PANEL
PLY	- PLYWOOD SHEATHING
FRP	- TEXTURED FIBERGLASS REINFORCED PANEL
TL	- CERAMIC TILE / MATCHING GROUT
EC	- EPOXY SURFACE COATING
SC	- EXPOSED SEALED CONCRETE
CT	- CARPET TILE
VCT	- VINYL COMPOSITE TILE
LVT	- LUXURY VINYL TILE
LVPF	- VINYL PLANK FLOORING, COMMERCIAL GRADE, TEXTURED WOOD LOOK
SACT	- SUSPENDED ACOUSTIC CEILING TILE, COMMERCIAL GRADE

LEGEND

EXIT	ILLUMINATED EXIT SIGN
102	DOOR NUMBER
○	WINDOW TYPE
◇	WALL TYPES SEE A111
FE	FIRE EXTINGUISHER
OR	EMERGENCY LIGHTS
FP	FIRE ALARM PULL STATION
○	SMOKE DETECTOR
○	HEAT DETECTOR
○	MONOXIDE DETECTOR
101	ROOM NUMBER
HEC	HORN STROBE ? CANDELA CEILING MOUNTED
HEC	HORN STROBE ? CANDELA WALL MOUNTED
HEC	STROBE ONLY 15 CANDELA WALL MOUNTED
FD	FLOOR DRAIN
FD	AUTO. DOOR OPENER WALL CONTROL PAD

1 FIRST FLOOR
3/16" = 1'-0"

TRAVEL PATHS

PATH #	TOTAL	DEAD END LENGTH	COMMON PATH LENGTH	PATH DESCRIPTION
TP1	81'	N/A	26'	FROM RM. 107 TO EXIT 101
TP2	109'	N/A	14'	FROM RM. 109 TO EXIT 101
TP3	-	N/A	-	-

ROOM SCHEDULE

NUMBER	NAME	AREA	Wall Finish North	Wall Finish East	Wall Finish South	Wall Finish West	CEILING FINISH	CEILING HEIGHT	BASE	FLOOR FINISH	COMMENTS
101	APPARATUS BAY	3283 SF	MP	MP	MP	MP	SEE REFLECTIVE CEILING PLAN	16'-0"	-	SC	
102	HALL	354 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
103	MECHANICAL	116 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
104	DAY ROOM	156 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
105	CHIEFS OFFICE	128 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
106	WRR	79 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
107	MRR	85 SF	GWB	GWB	GWB	GWB	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	LVT	
108	NOT USED	Not Placed									
109	CASCADE	82 SF	GWB OVER 1/2" PLYWOOD	GWB OVER 1/2" PLYWOOD	GWB OVER 1/2" PLYWOOD	GWB OVER 1/2" PLYWOOD	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	SC	
110	GEAR/ DECON/ LAUNDRY	273 SF	FRP	FRP	FRP	FRP	SEE REFLECTIVE CEILING PLAN	8'-6"	RC	SC	

REVISIONS

NO.	DATE	DESCRIPTION	REV.	PER.	FM.
1	1/9/23				

PROJECT NAME: TOWN OF SWEDEN PROPOSED FIRE STATION
SHEET NAME: FLOOR PLAN

CLIENT: TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

DESIGNED BY: RGM
DRAWN BY: RGM
CHECKED BY: KGE
APPROVED BY: Approver
DATE: 12/14/22
SCALE: As Indicated
DATE ISSUED: 12/14/22

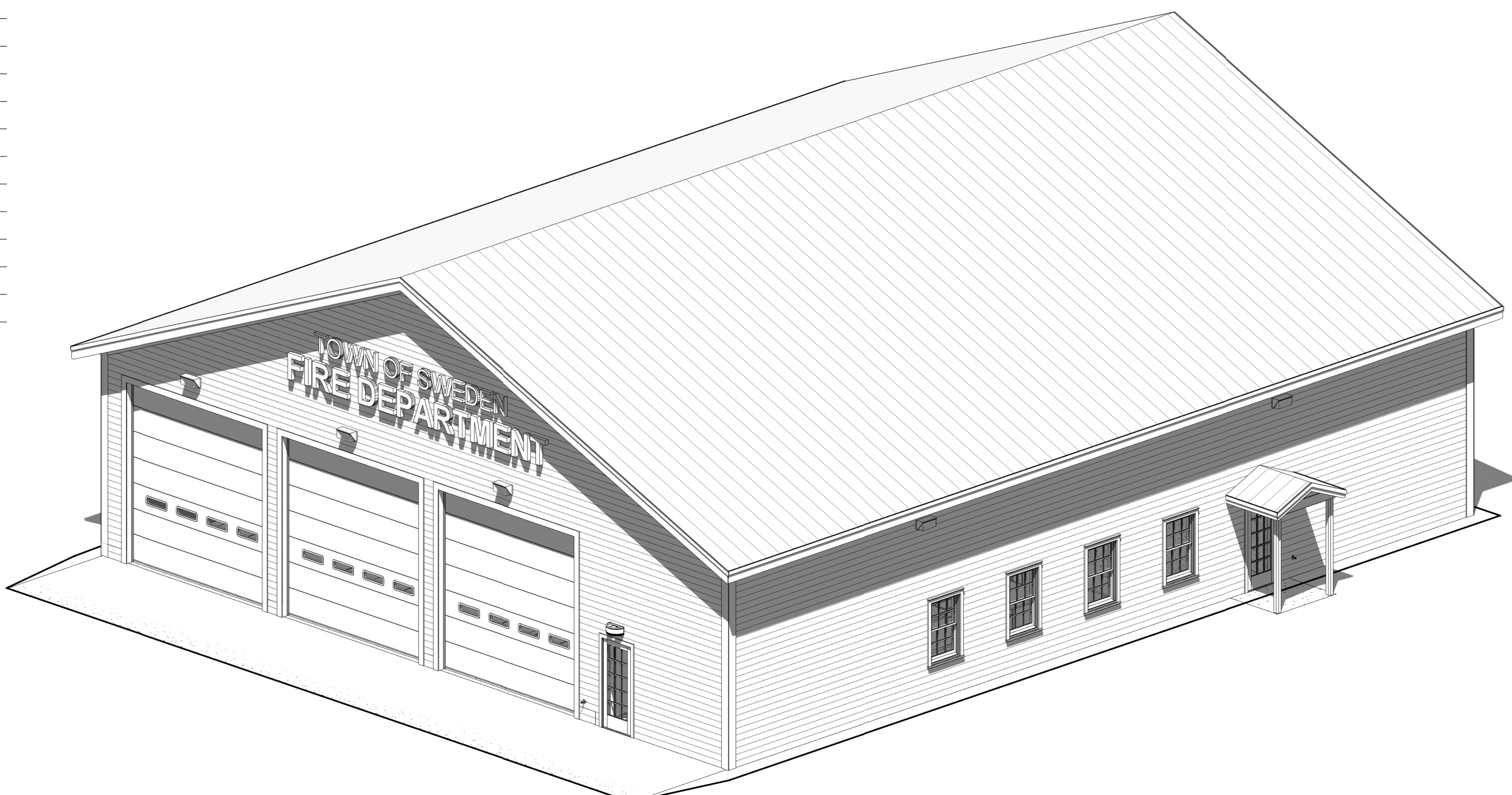
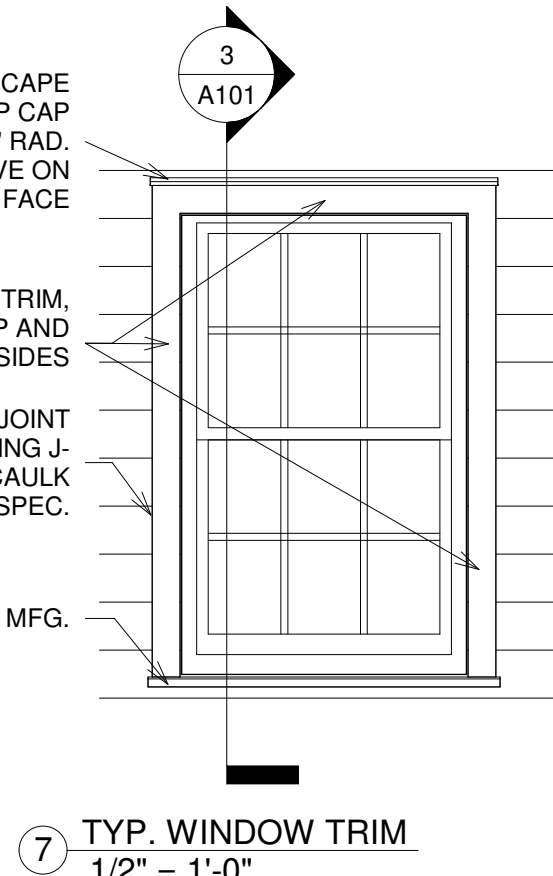
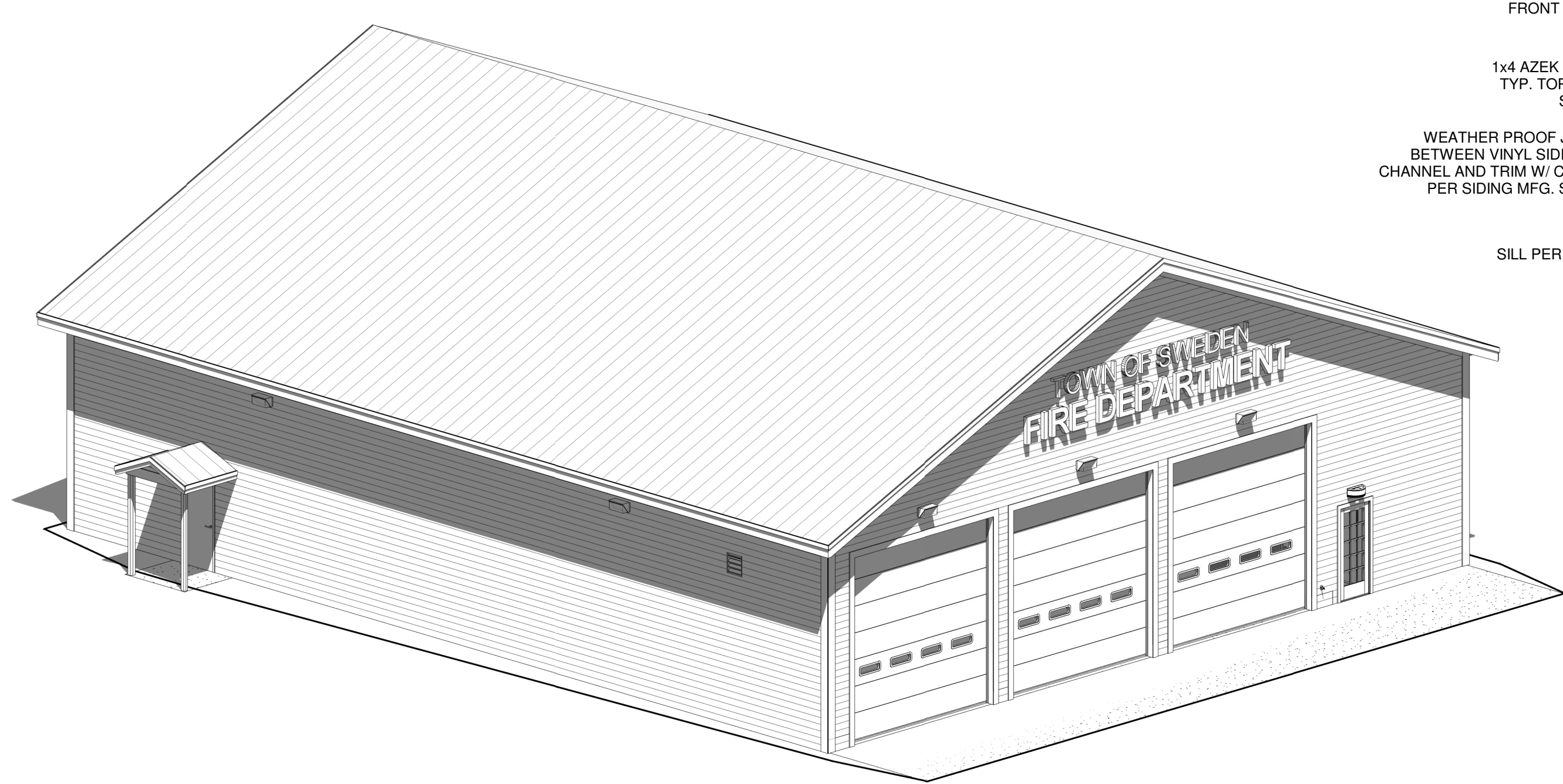
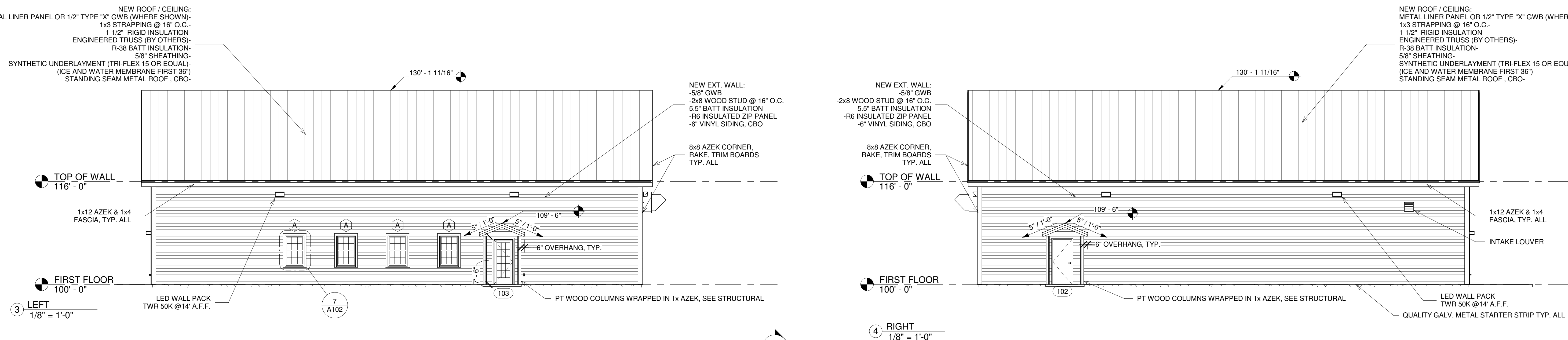
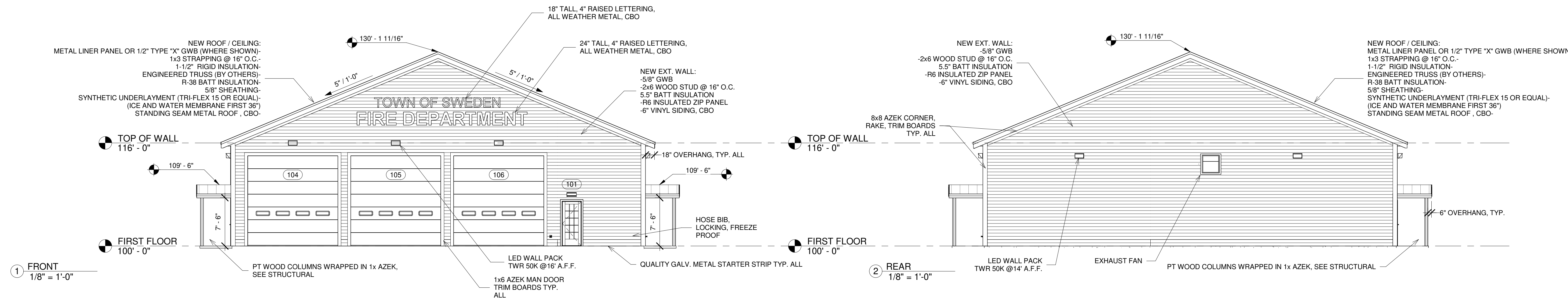
Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

STATE OF MAINE
Professional Seal: Keith C. [Signature]
LICENSED PROFESSIONAL ENGINEER
DRAWINGS NOT SEALED ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

SHEET A101 OF 16

A101

1/10/2023 2:49:06 PM J:\2019\19080 - Sweden Fire Station - Sweden - Town of Sweden\J. Drawings\BUILDING\19080A.rvt



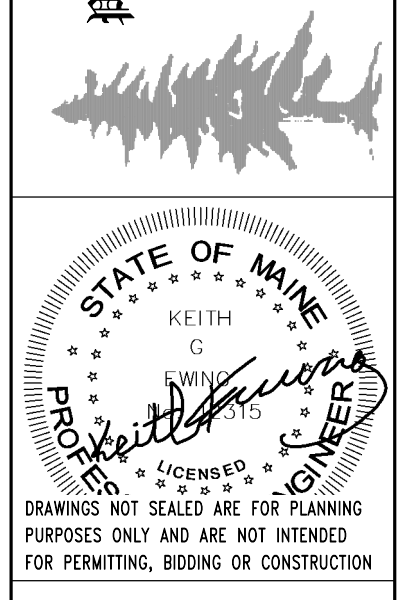
NO.	DATE	DESCRIPTION	DRWN	APPD.

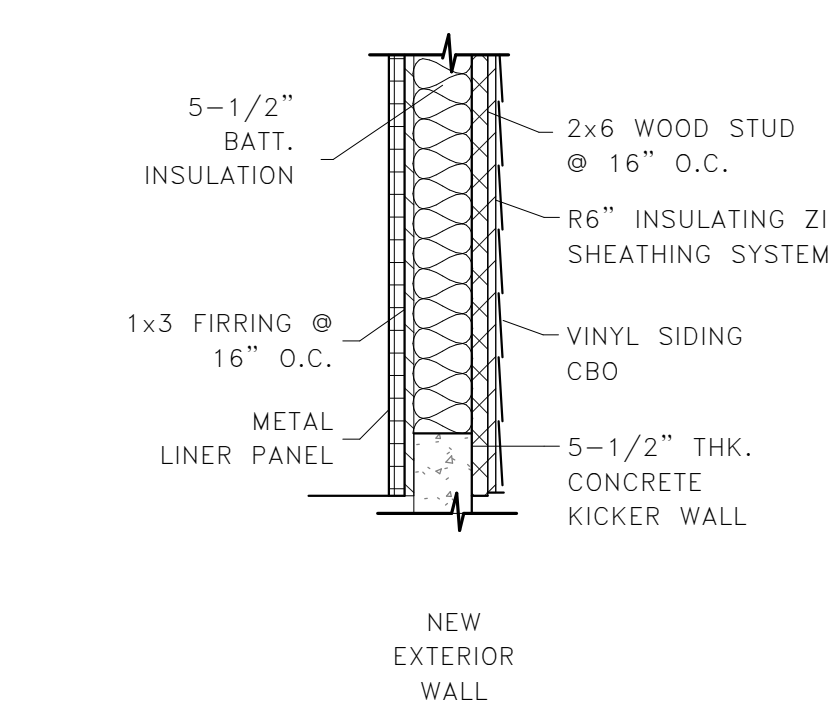
PROJECT NAME: TOWN OF SWEDEN
PROPOSED FIRE STATION
SWEDEN
SHEET NAME: ELEVATIONS

PROJECT # 19080
DRAWING # 19080
DESIGNER: RGM
CHECKED: ASV/KGE
APPROVED: Approver
PLAN DATE: 12/14/22
SCALE: As indicated
DATE ISSUED: 12/14/22

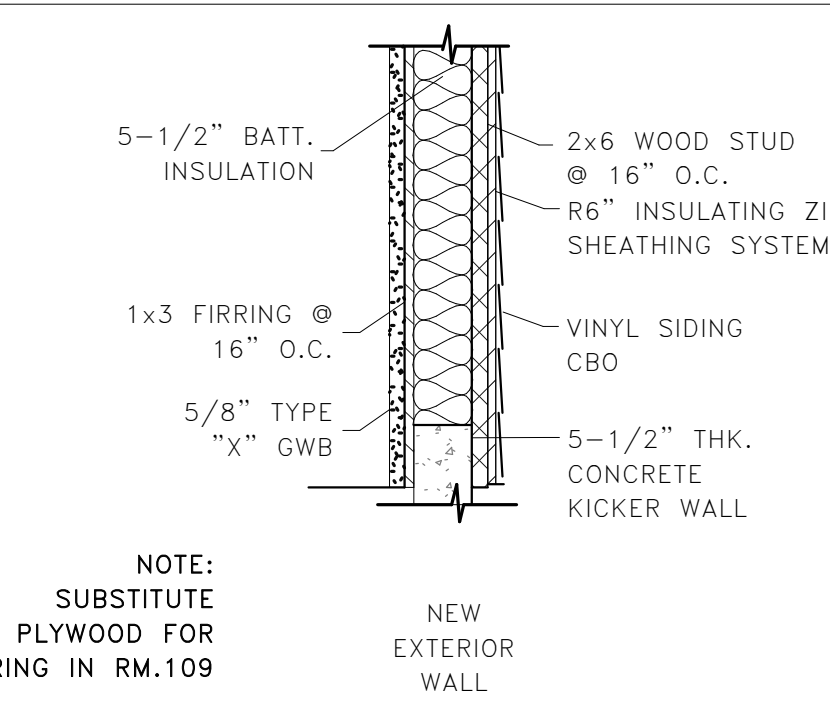
CLIENT & OWNER:
TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

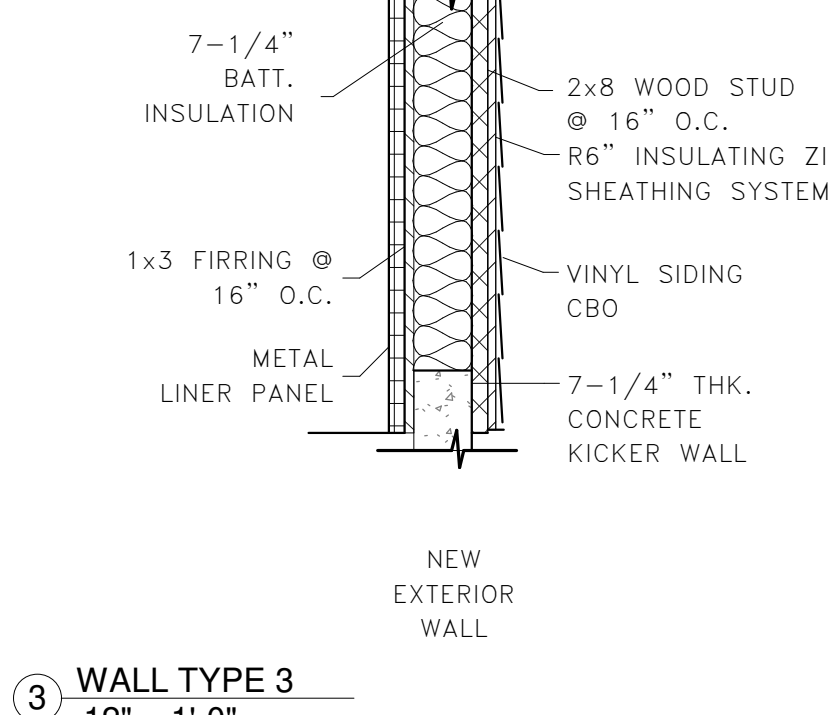




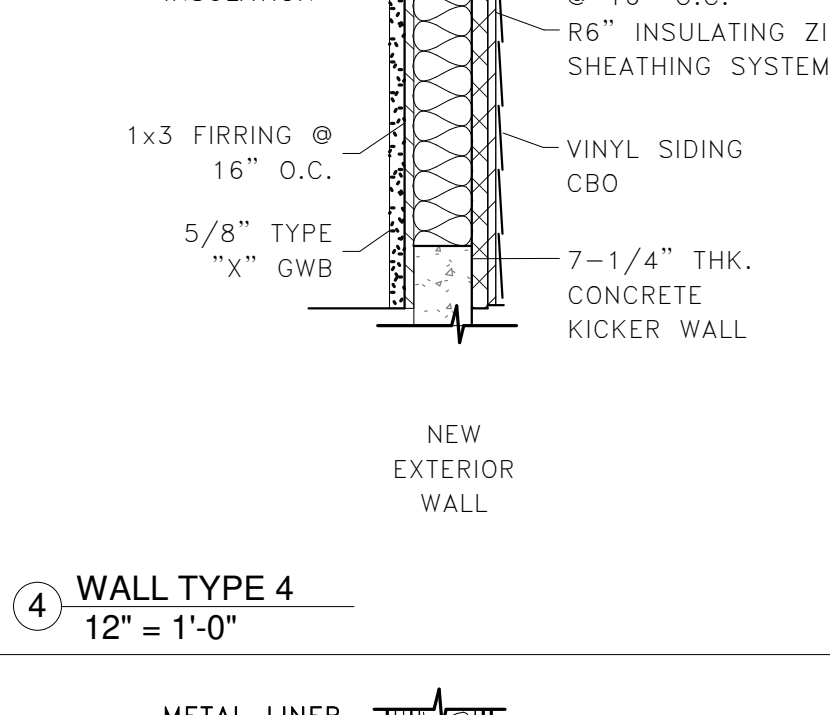
1 WALL TYPE 1
12" = 1'-0"



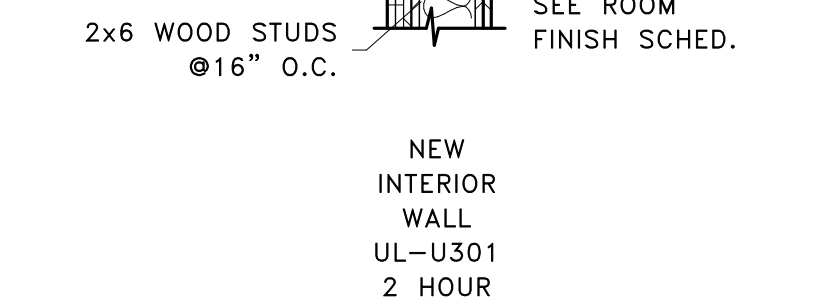
2 WALL TYPE 2
12" = 1'-0"



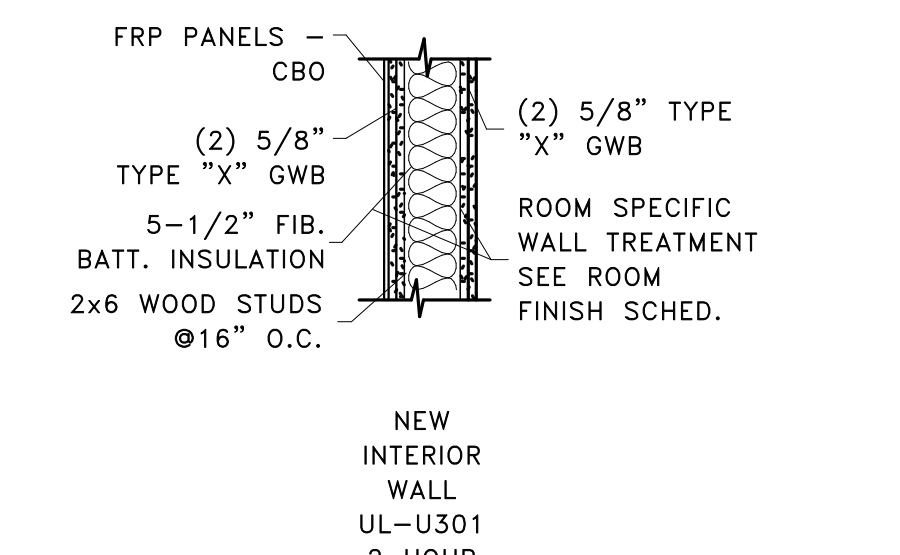
3 WALL TYPE 3
12" = 1'-0"



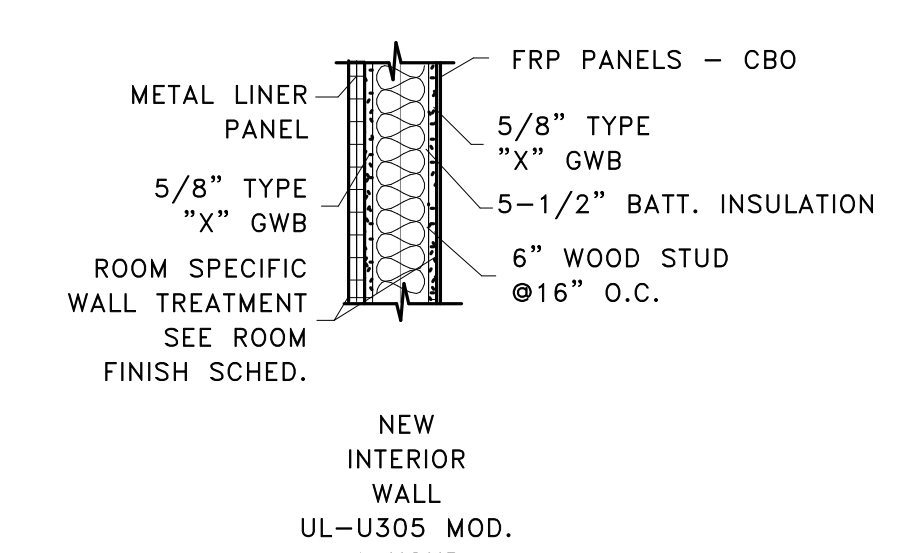
4 WALL TYPE 4
12" = 1'-0"



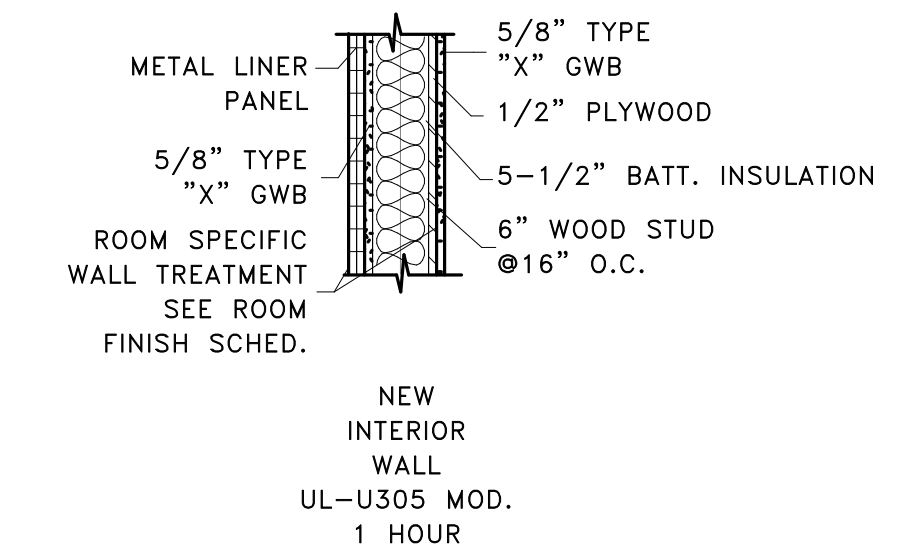
5 WALL TYPE 5
12" = 1'-0"



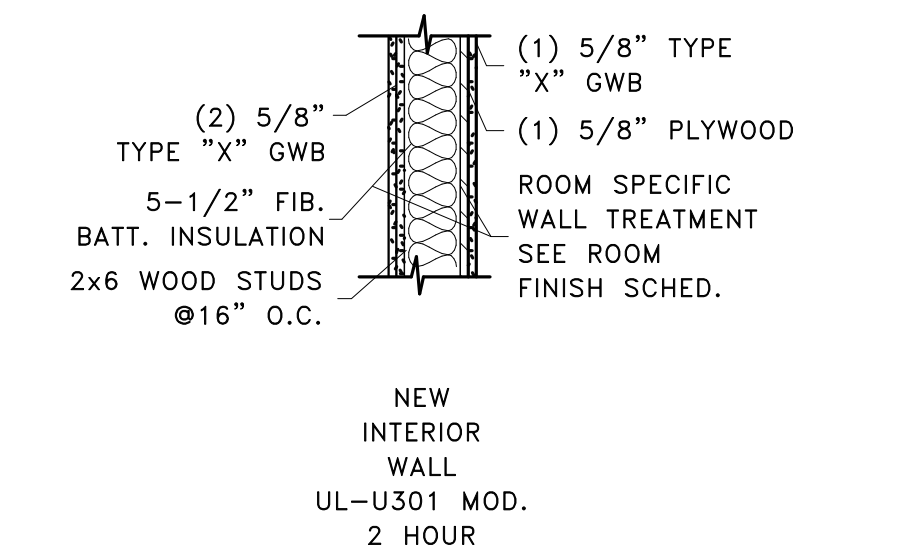
6 WALL TYPE 6
12" = 1'-0"



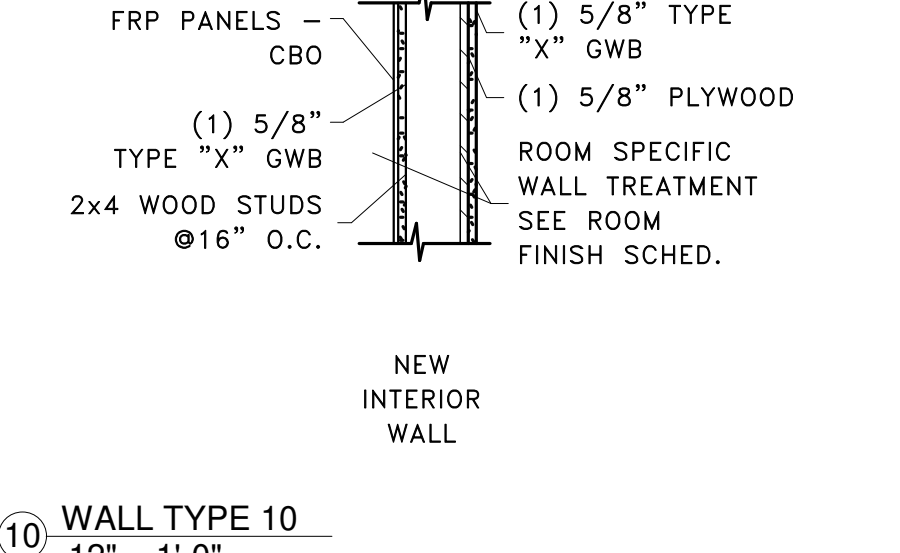
7 WALL TYPE 7
12" = 1'-0"



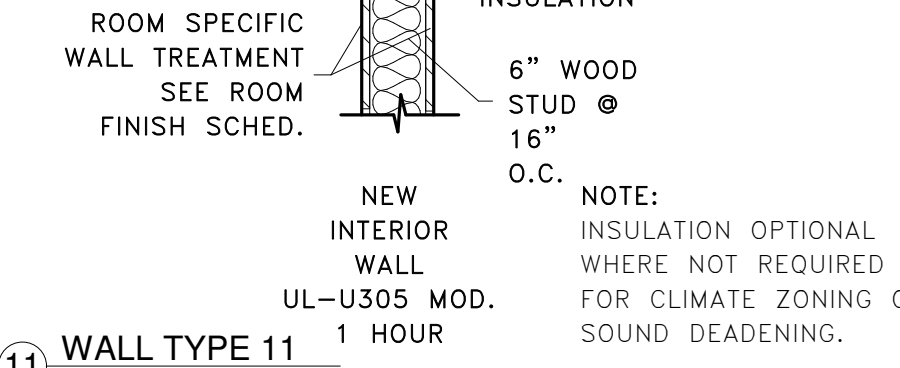
8 WALL TYPE 8
12" = 1'-0"



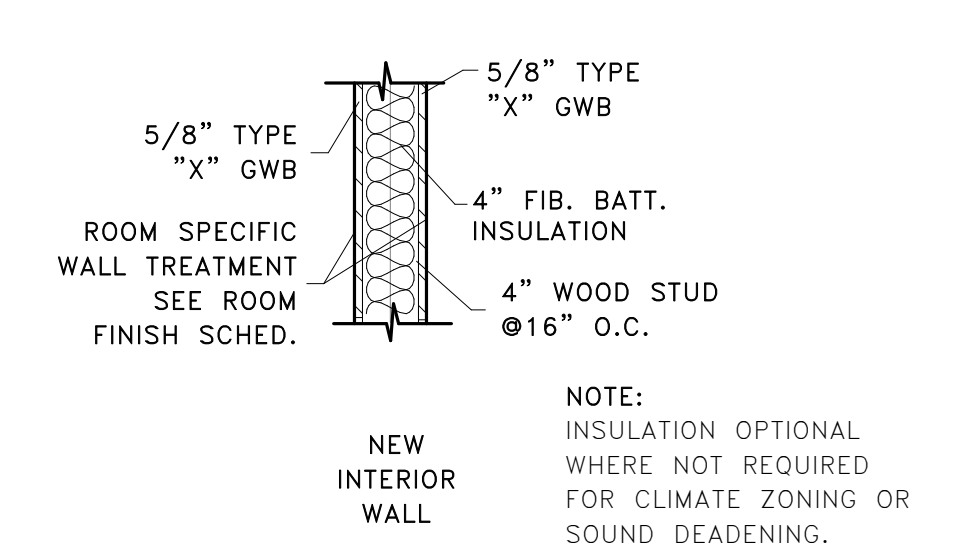
9 WALL TYPE 9
12" = 1'-0"



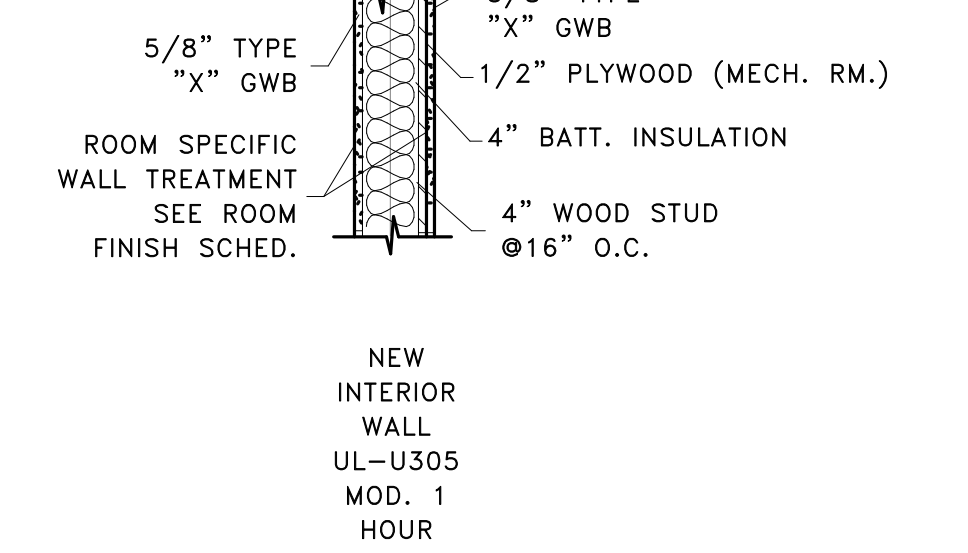
10 WALL TYPE 10
12" = 1'-0"



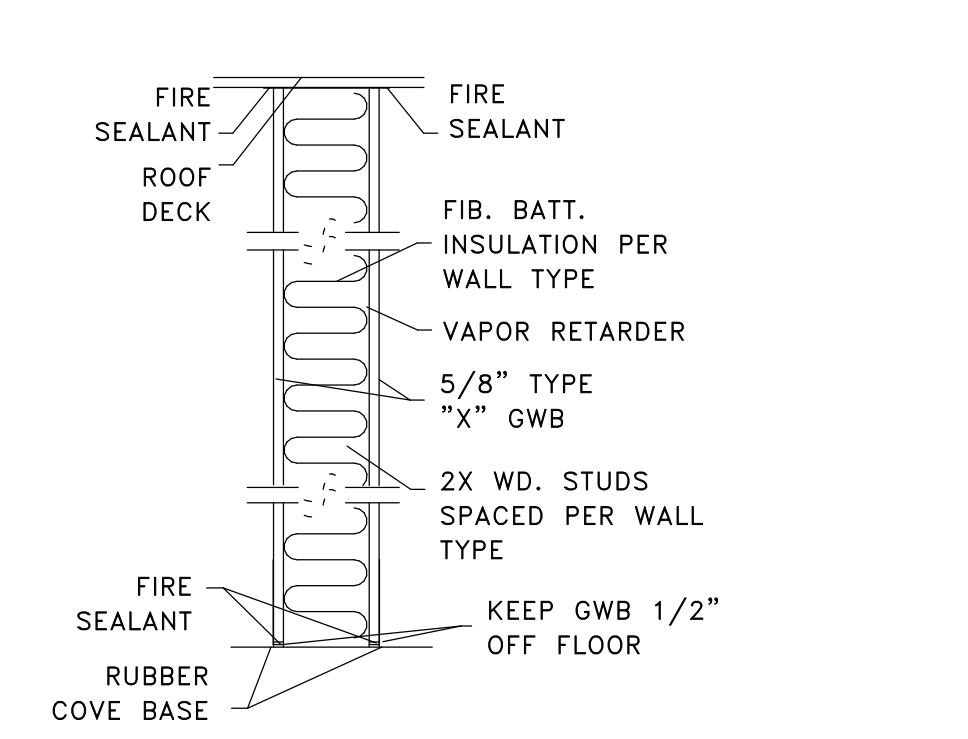
11 WALL TYPE 11
12" = 1'-0"



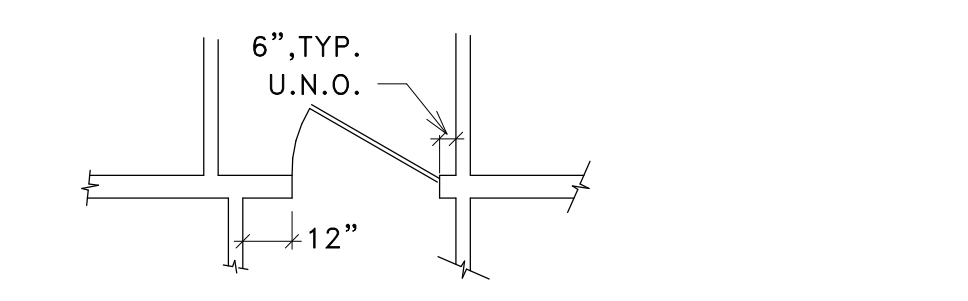
12 WALL TYPE 12
12" = 1'-0"



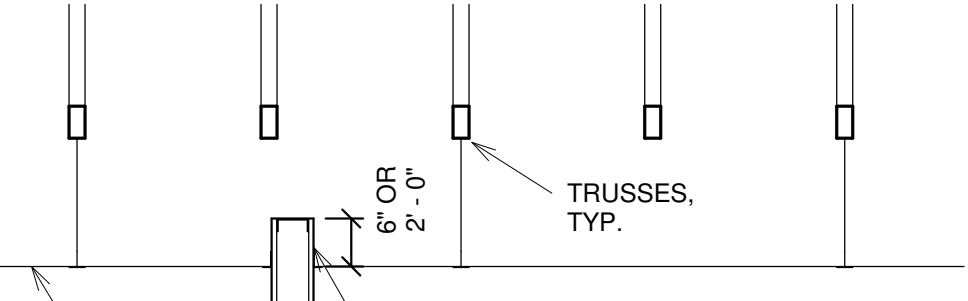
13 WALL TYPE 13
12" = 1'-0"



WALL TOP & BOTTOM DETAIL (ALL FIREWALLS)

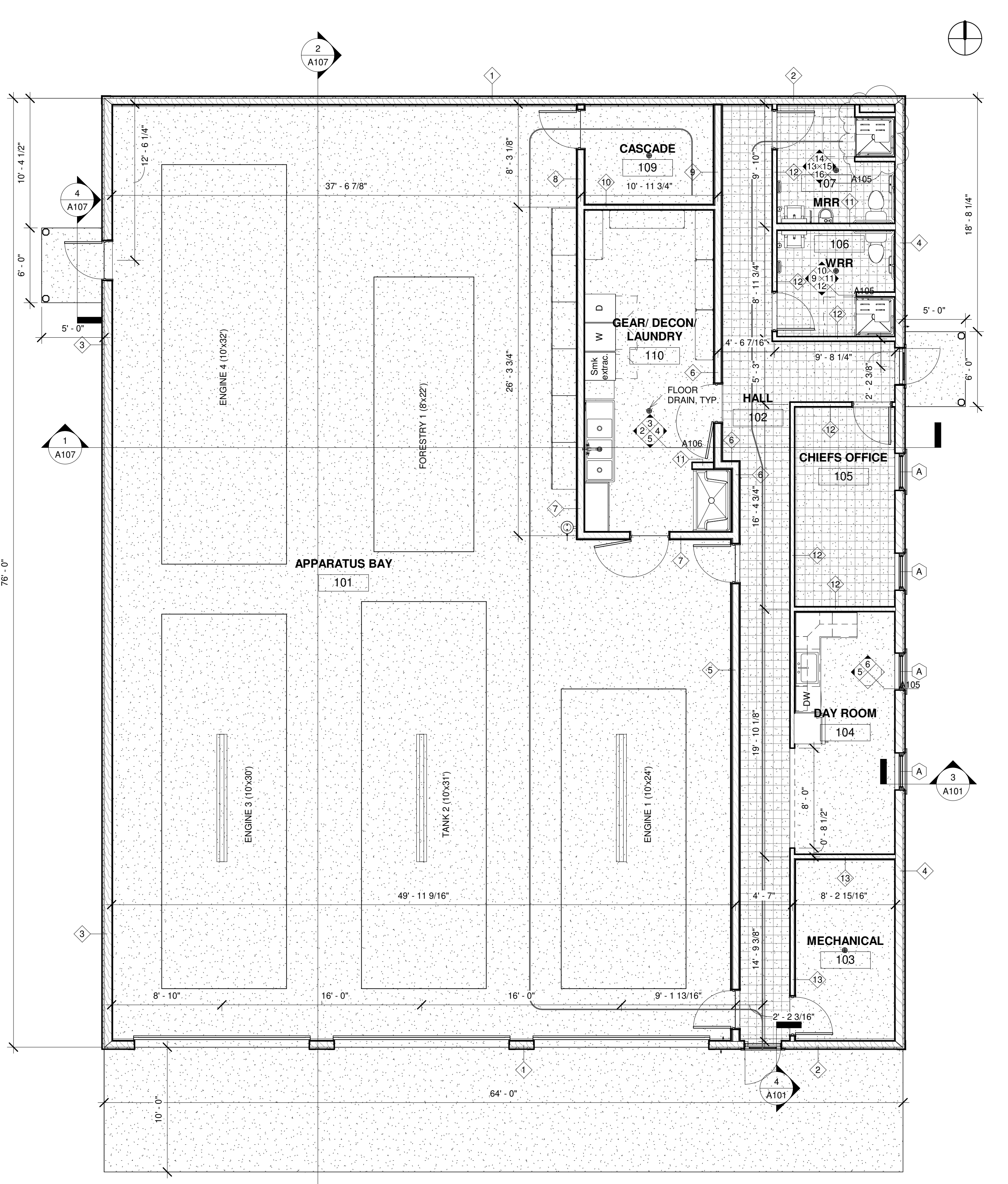


DOOR CLEARANCES
NOT TO SCALE
**MINIMUM DISTANCES REQUIRED ON ALL DOORS



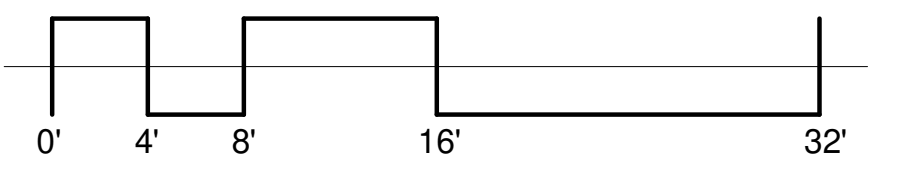
WALL HEIGHT DETAIL
NOT TO SCALE

NOTE:
ALL FIRE RATED WALLS ARE FULL HEIGHT (FLOOR TO TRUSS)
SEE WALL TYPES BELOW AND ROOM FINISH SCHEDULE ON PLAN A101.



14 FIRST FLOOR LAYOUT PLAN
3/16" = 1'-0"

- GENERAL LAYOUT NOTES:
- ALL INTERIOR DIMENSIONS ARE TO CENTER OF INTERIOR STUD FRAME AND INSIDE FACE OF EXTERIOR WALL STUD U.N.O.
 - ALL EXTERIOR DIMENSIONS ARE TO FACE OF EXTERIOR WALL STUD AND/OR FOUNDATION, U.N.O.
 - ALL DOOR OPENINGS ARE 6" FROM THE HINGE SIDE TO ADJACENT WALL, U.N.O.



NO.	DATE	DESCRIPTION	REV.	PER	FM
1	1/9/23				

REVISIONS

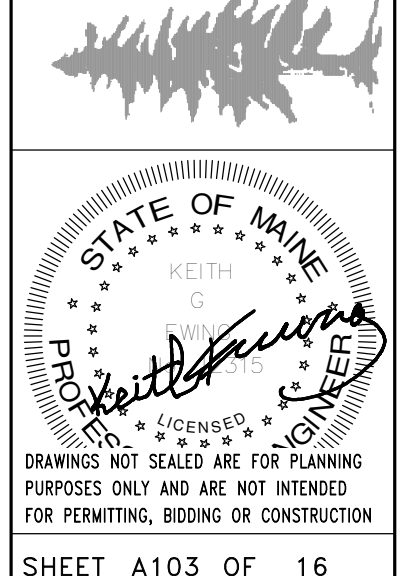
TOWN OF SWEDEN
PROPOSED FIRE STATION
SWEDEN
PROJECT NAME

DESIGNED BY: Designer
DRAWN BY: Author
CHECKED BY: Checker
APPROVED BY: Approver
PLAN DATE: 12/14/22
DATE ISSUED: 12/14/22


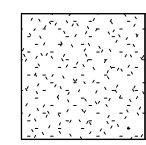
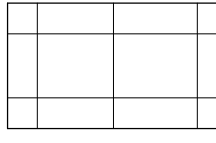
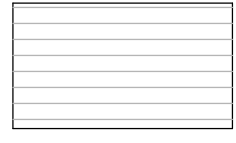
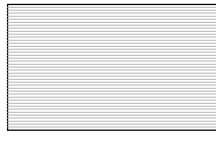
PROJECT # 19080
DRAWING # 19080
SCALE: As Indicated

CLIENT & OWNER
TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com



LEGEND

-  EXPOSED TO STRUCTURE OR EXISTING TO REMAIN
-  G.W.B: 5/8" -TYPE X- FIRE CODE U.N.O.
-  2x2 ACOUSTICAL TILE
ACT: ARMSTRONG WORLD INDUSTRIES,
MODEL: ULTIMA W/ HEALTH GUARD 1936 SIZE: 24"x24"x3/4", CLASS A, NRC: 0.70, CAC: MIN. 38. GRID: ARMSTRONG WORLD INDUSTRIES, MODEL: SUPRAFINE, EXPOSED GRID WIDTH: 9/16"
-  PERFORATED VINYL SOFFIT CERTAINTED,
MODEL: UNIVERSAL, PERFORATED, 4" RIB COLOR: GBO WIDTH: VARIES
-  METAL LINER PANEL EVERLAST,
MODEL: PBR 26 GA. COLOR: DRD WIDTH: 36"

LEGEND - RCP
1/4" = 1'-0"

ROOM LIGHTING TYPE DESIGNATION:
SEE SPECIFICATIONS FOR REQUIREMENTS, LOCATIONS SHOWN DIAGRAMATICALLY
ACTUAL COUNT AND LOCATIONS DETERMINED BY ROOM LUMEN REQUIREMENTS.

UNDER EXTERIOR CANOPIES:
SURFACE MOUNT 6" ROUND LED DOWN LIGHTS, WET LOCATIONS

EXTERIOR LED WALL PACKS:
SEE ELEVATION FOR LOCATIONS

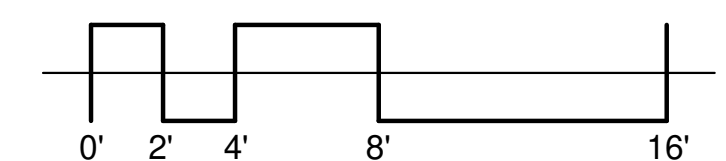
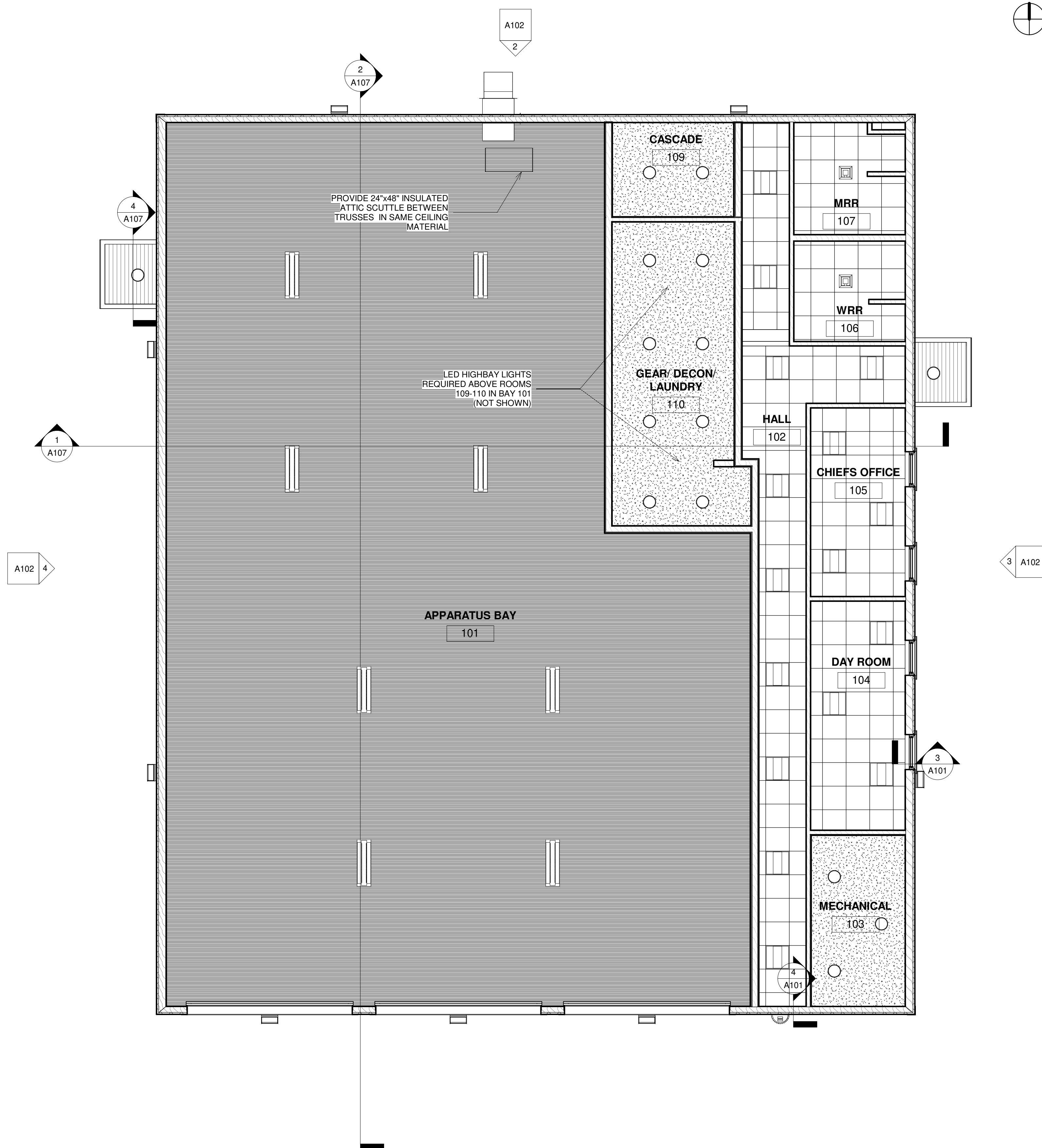
APPARATUS BAY 101:
SUSPENDED 14x48 LED HIGHBAY

ROOMS 103, 109, 110:
SURFACE MOUNT 6" ROUND LED DOWN LIGHTS, DIMMABLE, WET LOCATIONS

RESTROOMS 106, 107:
DROP-IN EXHAUST FAN / LED

ROOMS 102, 104, 105:
2x2 DROP-IN LED FOCAL POINT TROFFER

1 FIRST FLOOR
3/16" = 1'-0"

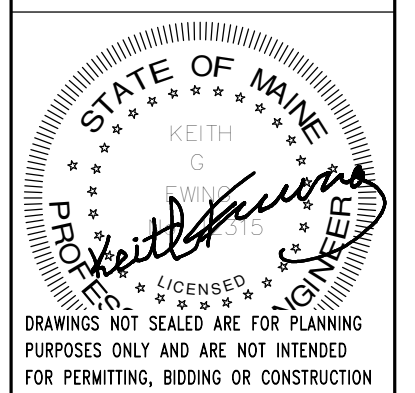


NO.	DATE	DESCRIPTION	DRWN	APPD.

PROJECT NAME: TOWN OF SWEDEN
PROPOSED FIRE STATION
SWEDEN, MAINE
SHEET NAME: REFLECTIVE CEILING PLAN

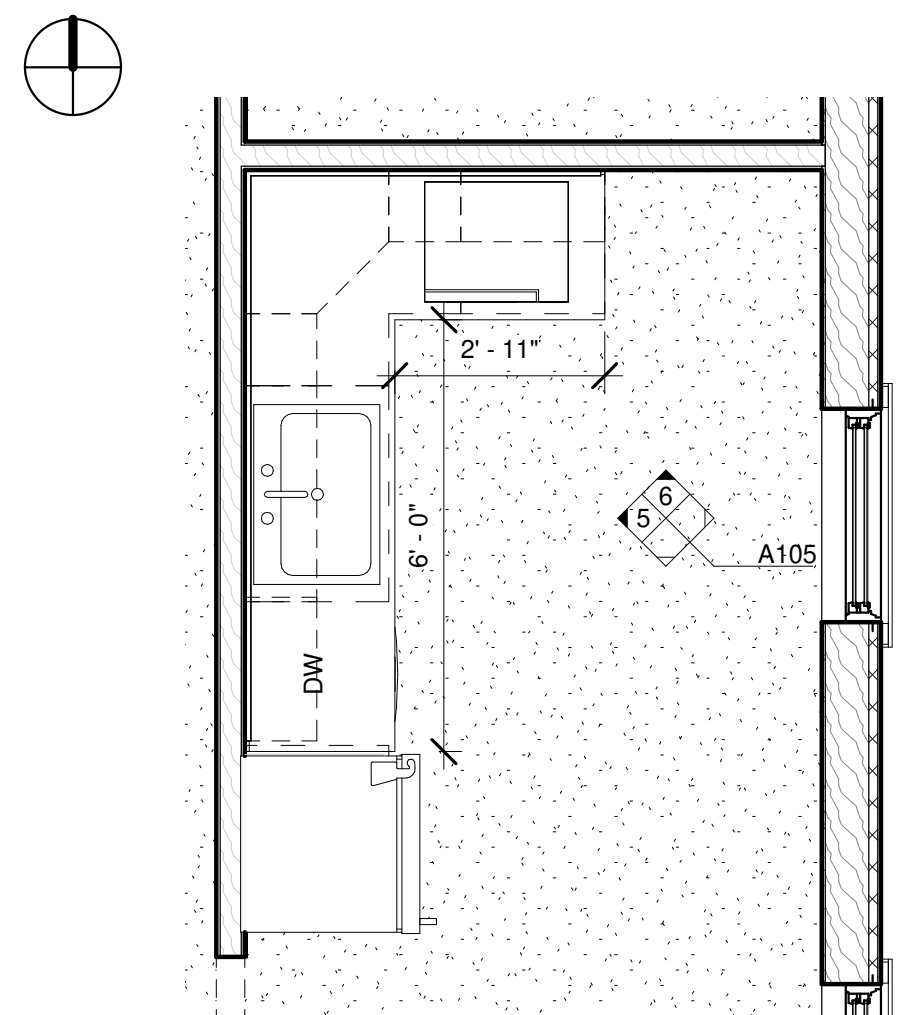
PROJECT #: 19080
DRAWING #: 19080
DESIGNED BY: Designer
DRAWN BY: Author
CHECKED BY: Checker
APPROVED BY: Approver
PLAN DATE: 12/14/22
DATE ISSUED: 12/14/22
SCALE: As indicated
CLIENT & OWNER: TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

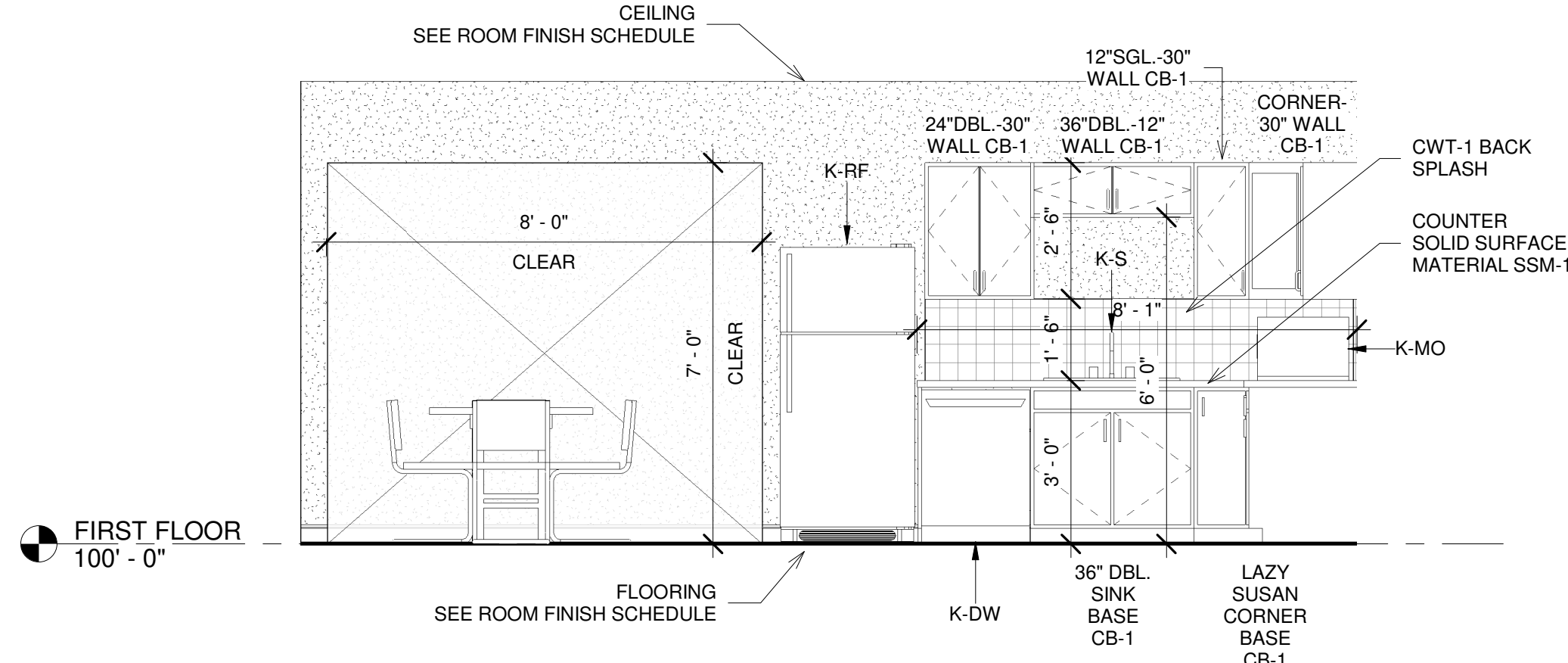


DRAWINGS NOT SEALED ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

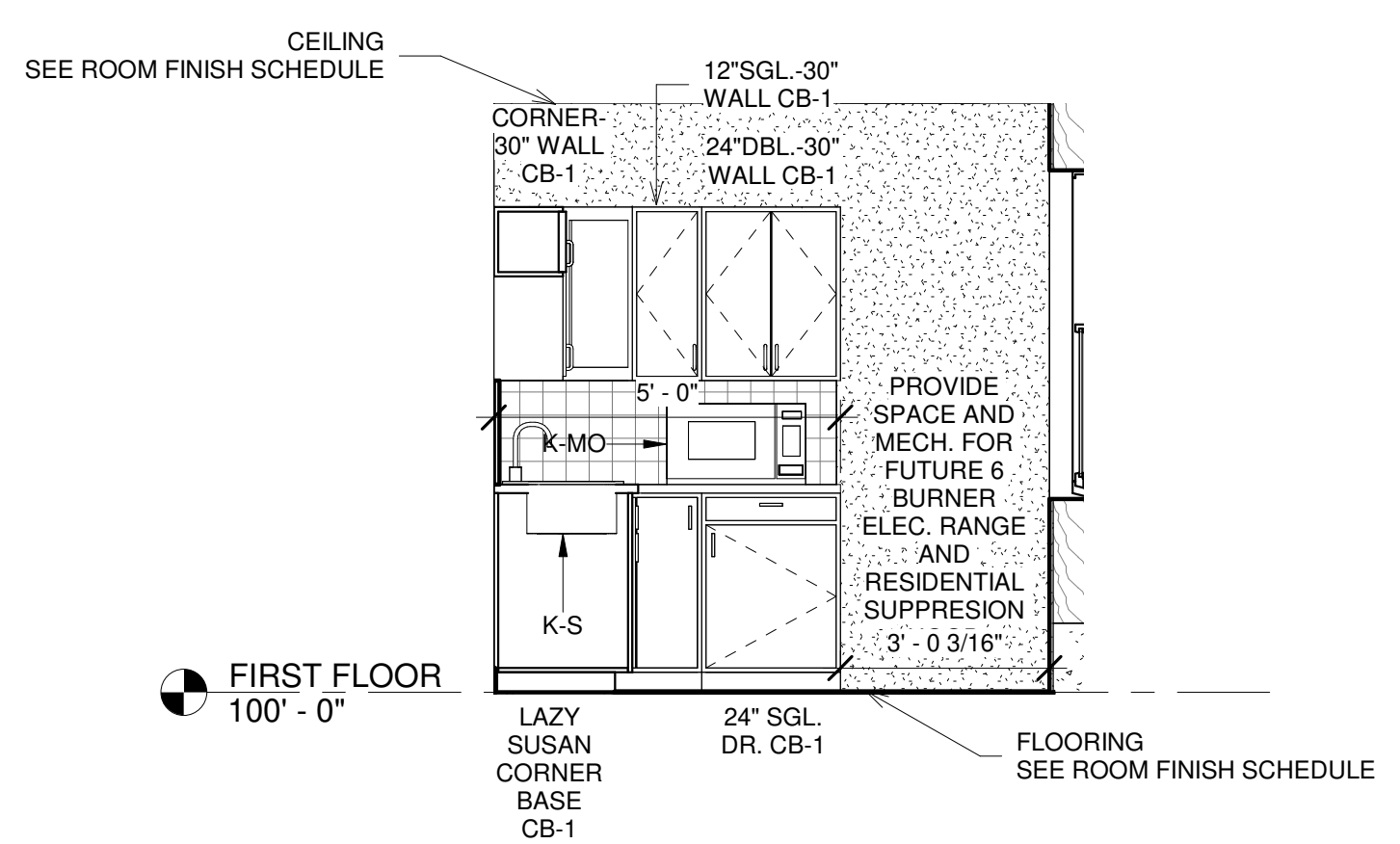
SHEET A104 OF 16
A104



1 FIRST FLOOR - RM. 104
3/8" = 1'-0"

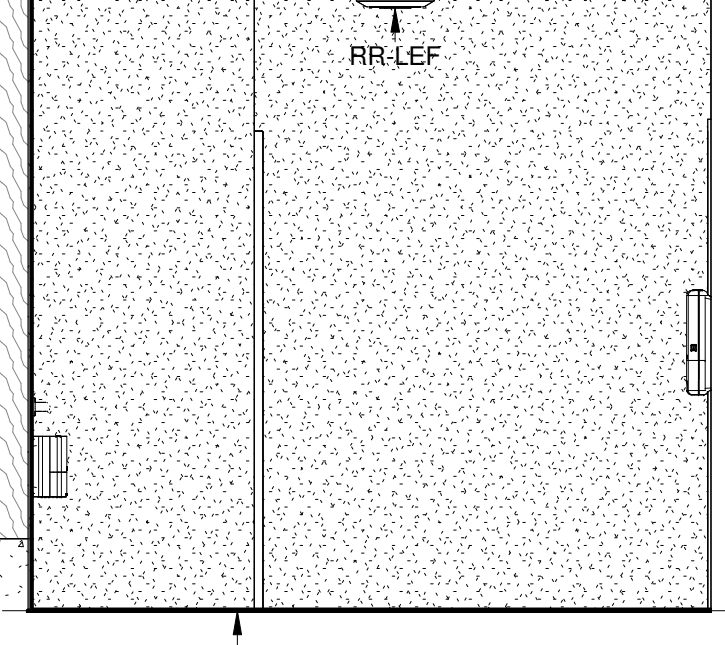


5 RM. 104 WEST
3/8" = 1'-0"

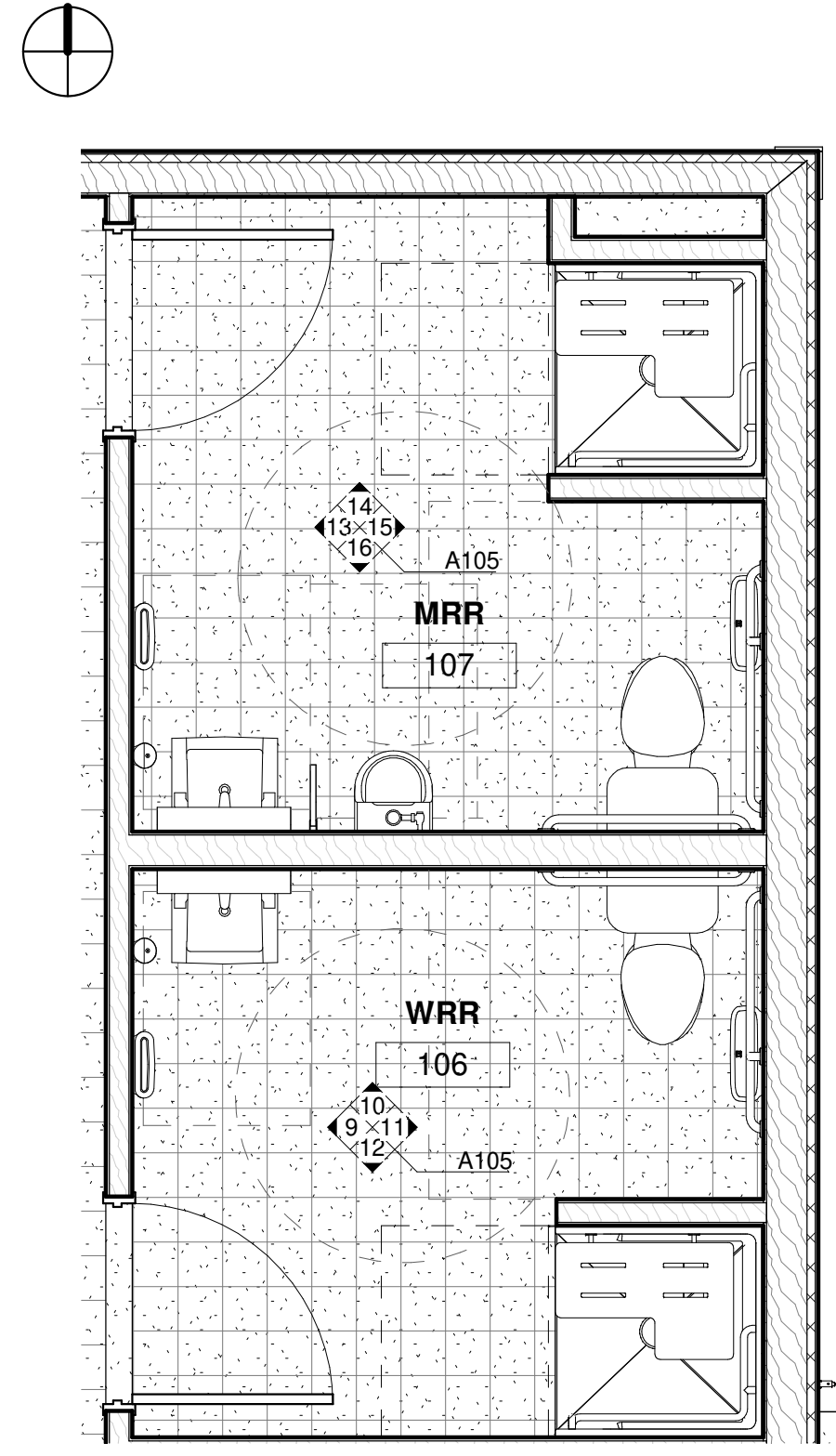


6 RM. 104 SOUTH
3/8" = 1'-0"

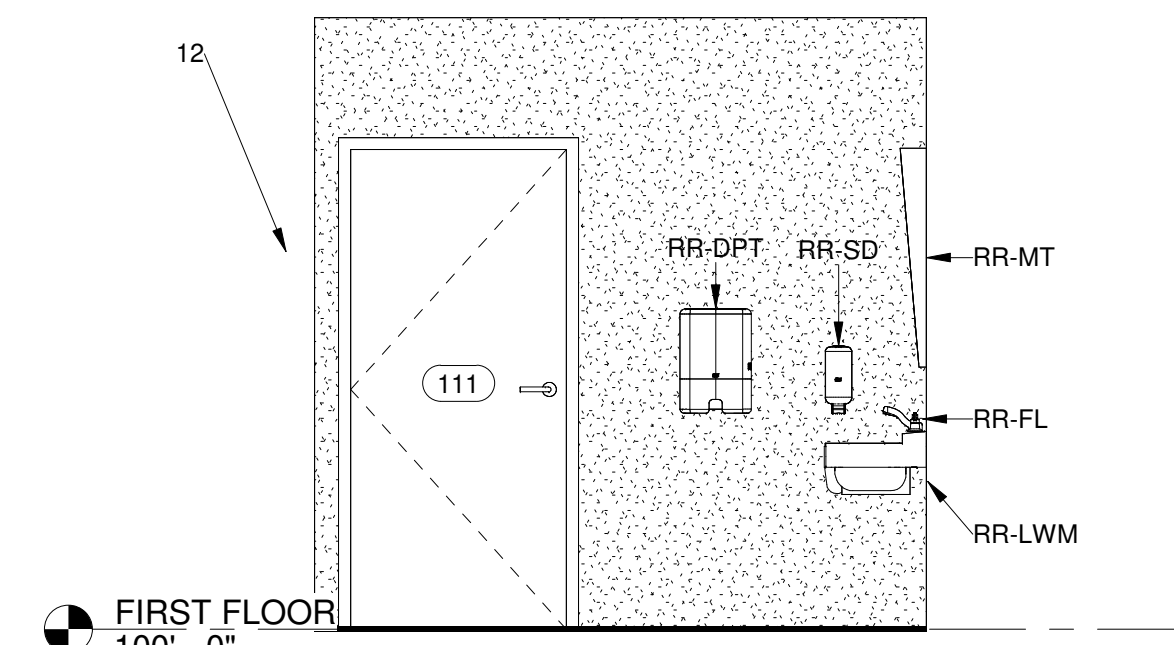
KITCHEN FIXTURES AND ACCESSORIES			
MARK	DESCRIPTION	CONTRACTOR INSTALLED	CONTRACTOR SUPPLIED
K-DW	DISHWASHER, 24" SS DOOR, 2 RACK, SS TUB	YES	NO
K-MO	MICROWAVE OVEN, COUNTERTOP	NO	NO
K-RF	FRIDGE, TOP FREEZER 21 CI. STAINLESS	NO	NO
K-S	SINK, DROP IN, 42"x21" DBL, SS	YES	YES



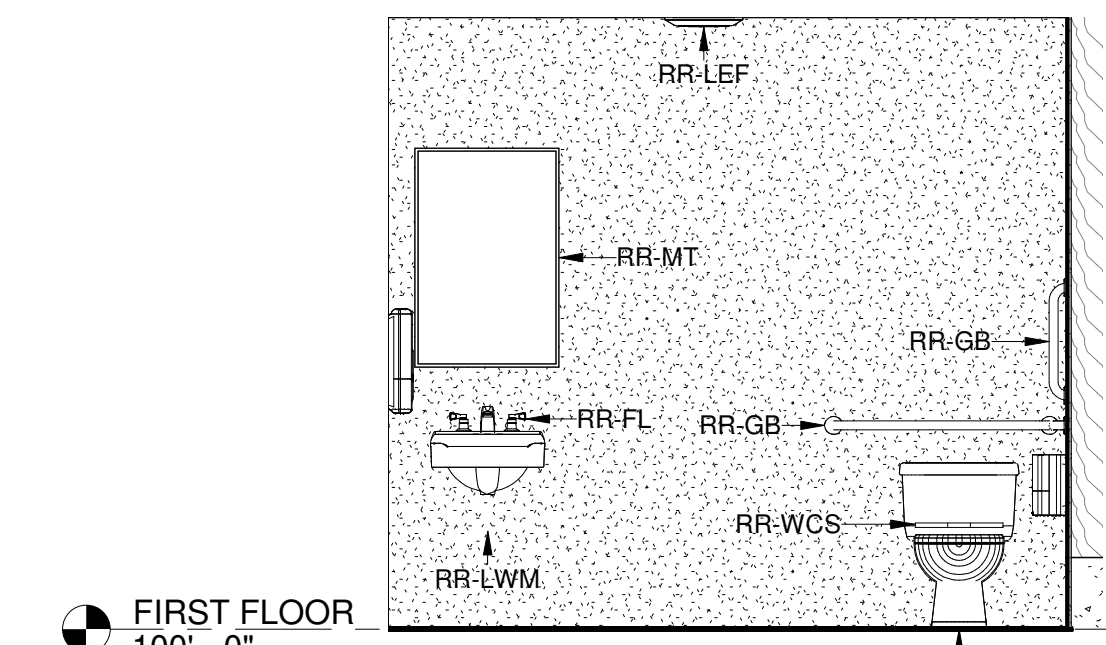
12 RM. 106-S
3/8" = 1'-0"



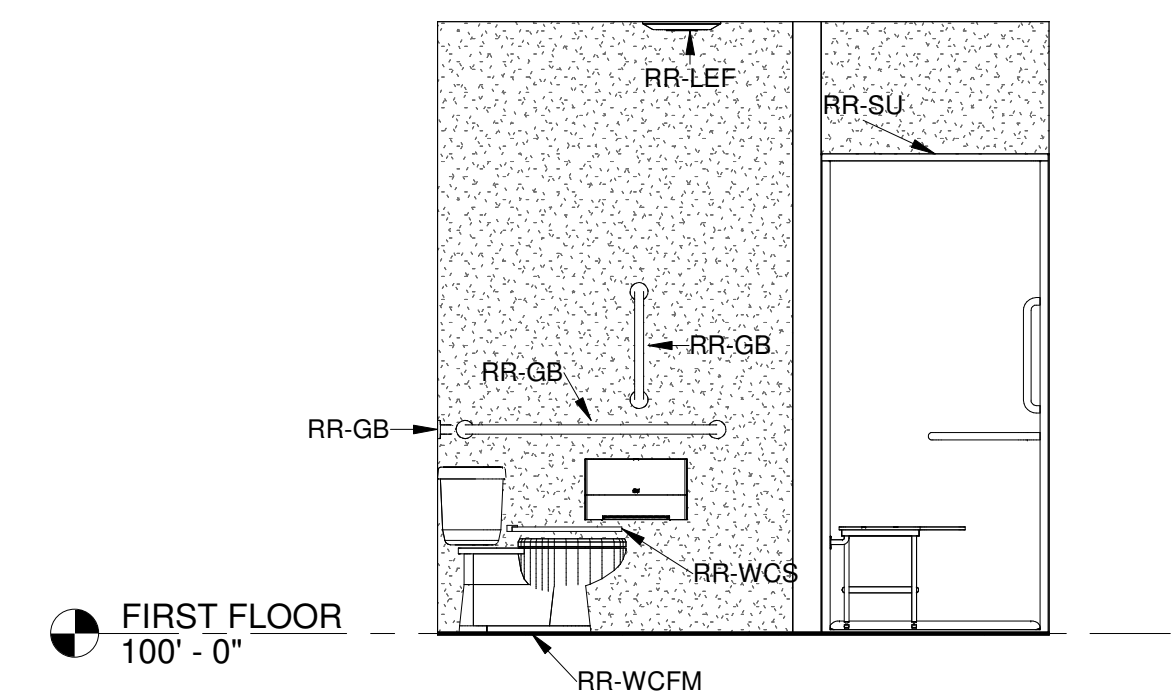
2 FIRST FLOOR - RMS. 106-107
3/8" = 1'-0"



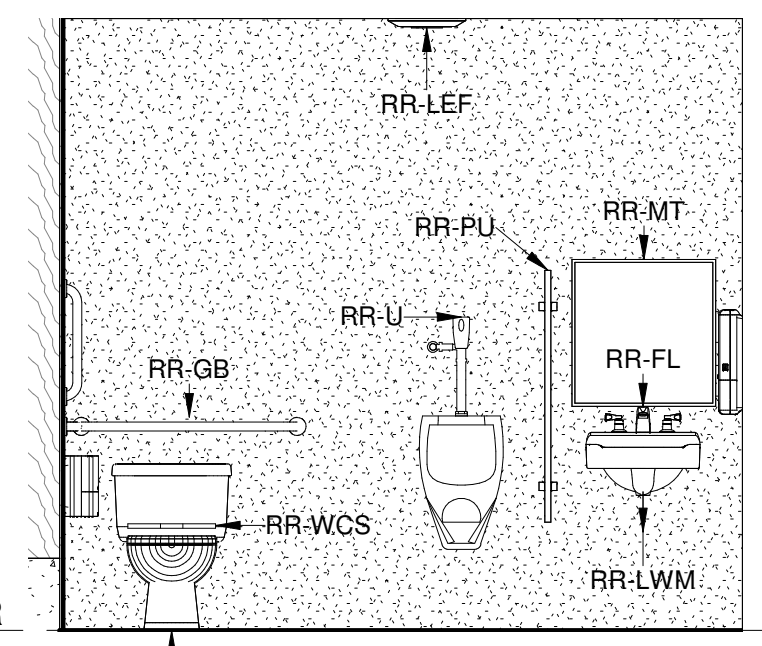
9 RM. 106-W
3/8" = 1'-0"



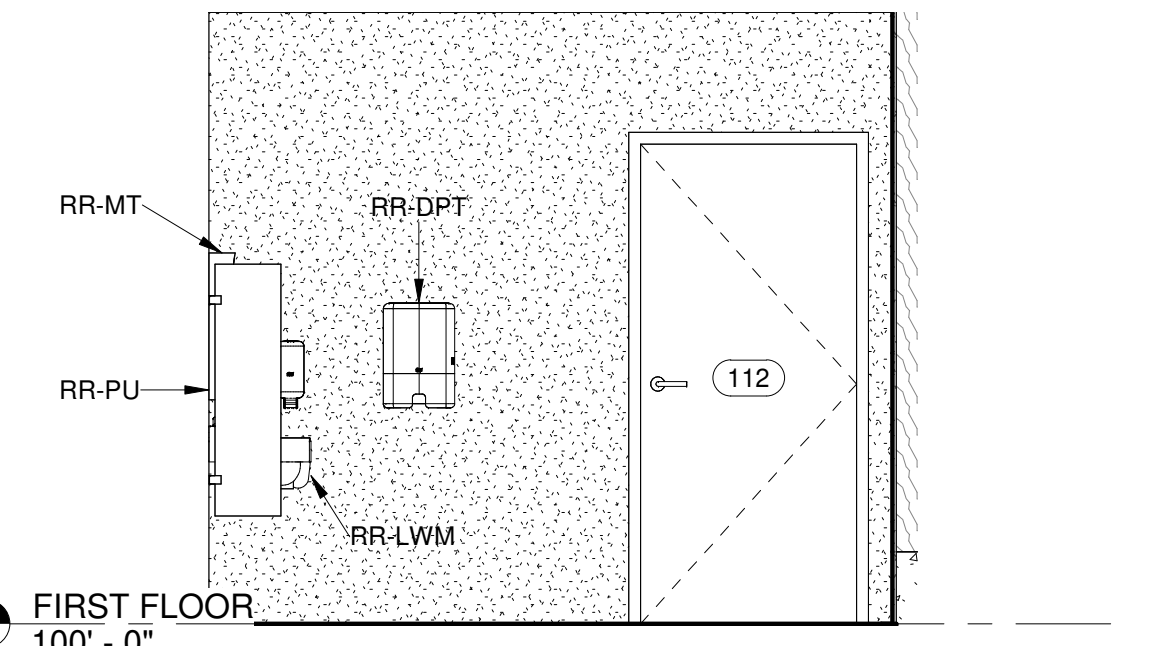
10 RM. 106-N
3/8" = 1'-0"



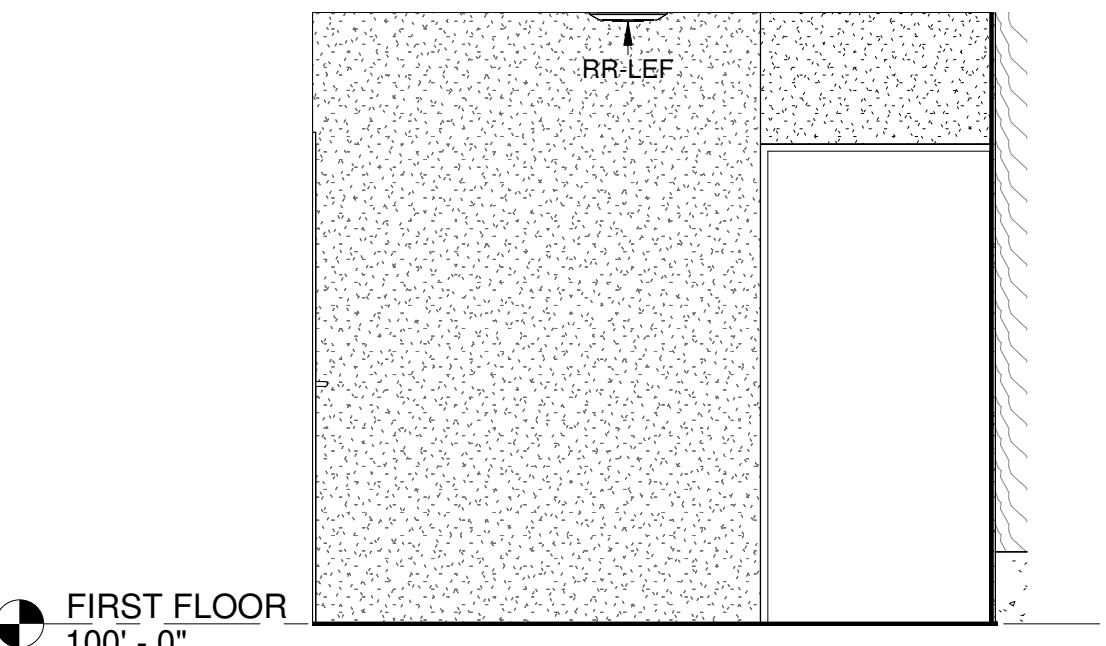
11 RM. 106-E
3/8" = 1'-0"



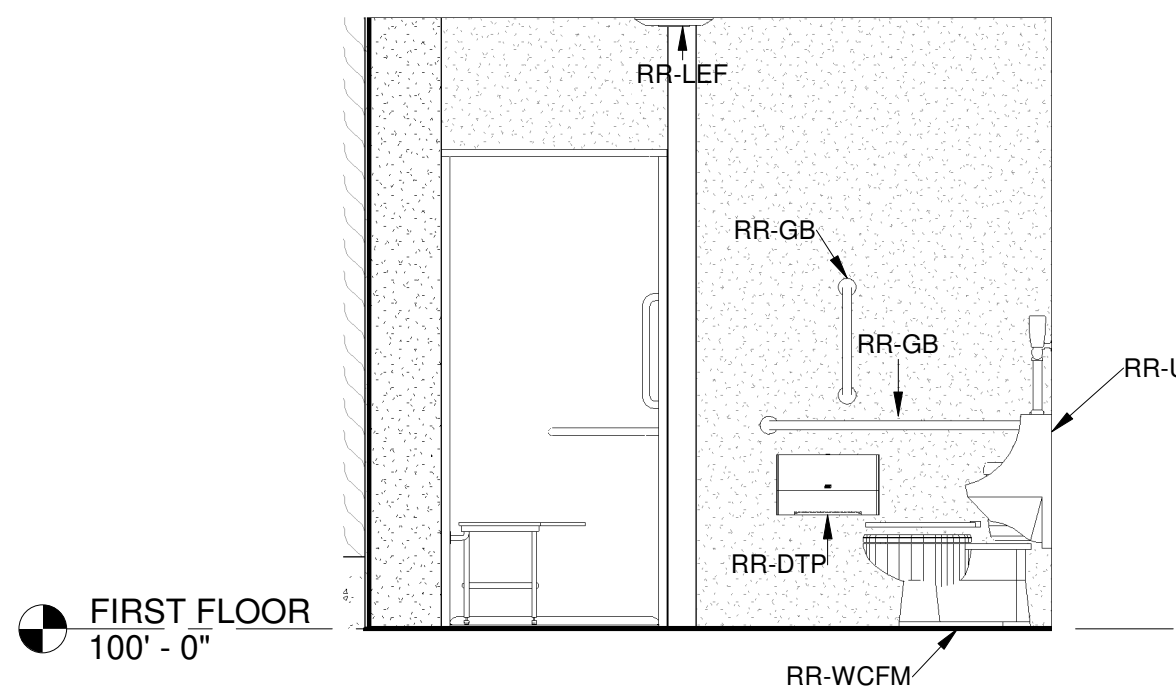
16 RM. 107S
3/8" = 1'-0"



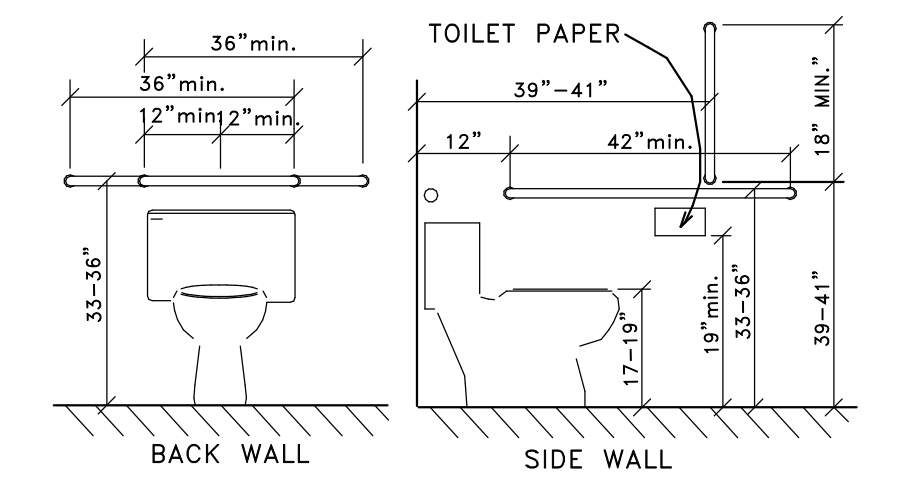
13 RM. 107-W
3/8" = 1'-0"



14 RM. 107-N
3/8" = 1'-0"



15 RM. 107-E
3/8" = 1'-0"



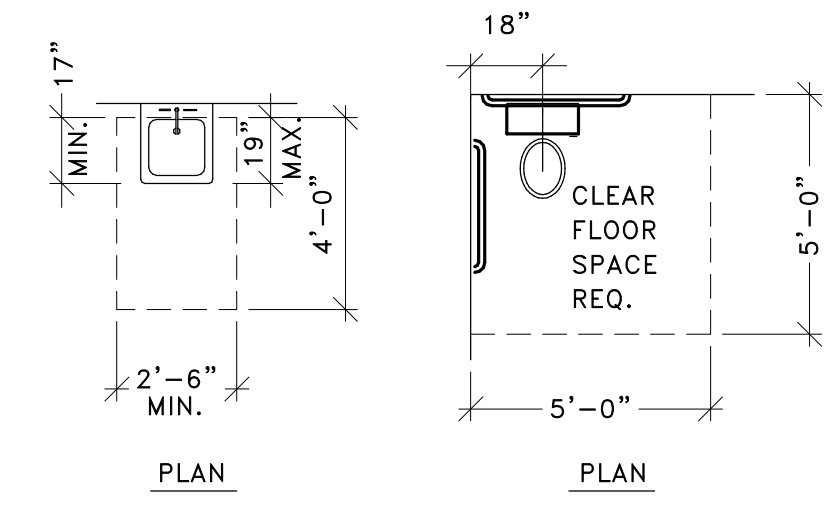
NOTE: ALL EXPOSED PIPING SHALL BE INSULATED

ACCESSORY	RESTROOM ACCESSORY SCHEDULE					
	106 WRR		107 MRR		-	
	W	E	S	W	W	E
ADA TOILET GRAB BARS	o	o	o	o		
TOILET PAPER DISPENSER	o		o			
PAPER TOWEL DISPENSER		o		o		
SANITARY NAPKIN DISPENSER						
CONDOM DISPENSER						
AIR HAND DRIER						
HAND SOAP DISPENSER		o		o		
ADA TILTED MIRROR	o			o		
STANDARD ABOVE SINK MIRROR						
WALL MOUNTED RECESSED TRASH BIN						
SANITARY NAPKIN DISPOSAL						
FOLDING CHANGING TABLE, WALL MOUNTED						

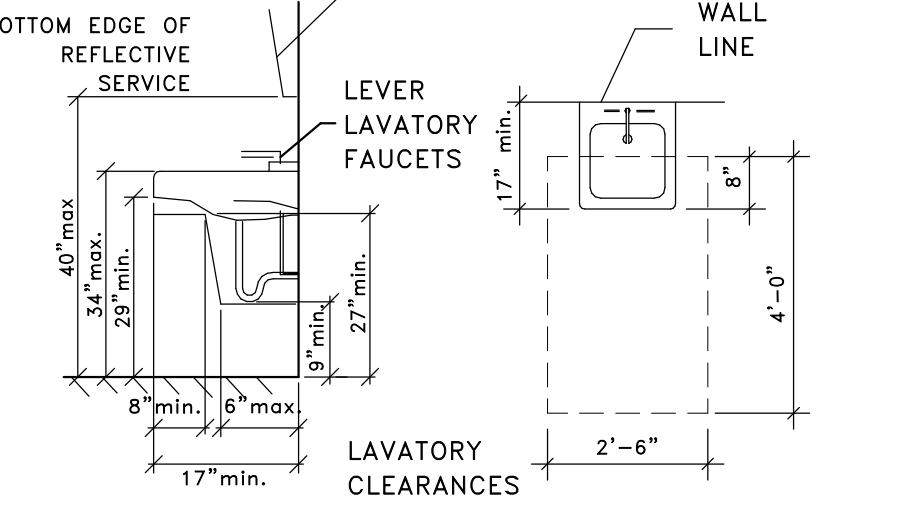
RESTROOM FIXTURES AND ACCESSORIES				
MARK	DESCRIPTION	CONTRACTOR INSTALLED	CONTRACTOR SUPPLIED	CONTRACTOR SUPPLIED, INSTALLED
RR-DPT	PAPER TOWEL DISPENSER, FOLDED SHEET, WALL MOUNTED, ADA COMPLIANT	YES	YES	
RR-DTP	TOILET PAPER DISPENSER, WALL MOUNTED, 2 ROLL, ADA COMPLIANT	YES	YES	
RR-FL	LAVATORY FAUCET, TWO HANDLE, ADA COMPLIANT	YES	YES	
RR-GB	GRAB BAR, BRUSHED SS, KNURLED FINISH, ADA COMPLIANT	YES	YES	
RR-LEF	LED LIGHT / EXHAUST FAN, 110 CFM MIN. 1SONE MAX.	YES	YES	
RR-LWM	LAVATORY, LEDGE TYPE WALL HUNG	YES	YES	
RR-MT	MIRROR, FIX TILT, WALL MOUNTED, ADA COMPLIANT	YES	YES	
RR-PU	PARTITION, URINAL HDPE, 18"x42"	YES	YES	
RR-SD	SOAP DISPENSER, FOAMY, WALL MOUNTED, ADA COMPLIANT	YES	YES	
RR-SU	TRANSFER TYPE SHOWER W/ SEAT, BARS, WAND AND CONTROLS, ADA COMPLIANT	YES	YES	
RR-U	URINAL, W/ MOTION FLUSH VALVE, ADA COMPLIANT	YES	YES	
RR-WCFM	1.28 gpi ELONGATED HIGH EFFICIENCY FLOOR MOUNT SIPHON JET WATER CLOSET	YES	YES	
RR-WCS	SEAT, OPEN FRONT, LIDLESS, ADA COMPLIANT	YES	YES	

TOILET ROOM ACCESSORY MOUNTING HEIGHTS (A.F.F.) (NOT ALL USED)

- GRAB BARS _____ 33" to 36"
- TOILET TISSUE HOLDER _____ 19" MINIMUM HT.
- TOWEL BAR or PAPER TOWEL DISPENSER _____ 48" MAXIMUM HT.
- PAPER TOWEL DISPOSAL _____ 48" TO OPENING OR MECHANISM
- SOAP DISPENSER _____ 48" MAXIMUM HT.
- SANITARY NAPKIN DISPOSAL _____ 19" MINIMUM HT. TO OPENING
- SANITARY NAPKIN DISPENSER _____ 48" TO MECHANISM
- MIRROR _____ 40" MAXIMUM HT. TO BOTTOM EDGE OF REFLECTIVE SURFACE
- SHelves or STORAGE CABINET _____ 48" MAXIMUM HT.
- SHOWER CONTROL _____ 48" MAXIMUM HT.
- LIGHT SWITCHES _____ 48" MAXIMUM HT.
- EXHAUST FAN SWITCH _____ 48" MAXIMUM HT.
- COAT HOOKS _____ 48" MAXIMUM HT.
- FIRE ALARM PULL _____ 48" MAXIMUM HT.
- THERMOSTAT _____ 48" MAXIMUM HT.
- URINAL _____ 17" MAXIMUM RIM HT.



BATHROOM FIXTURE CLEAR FLOOR SPACE
NOT TO SCALE



FIXTURE DETAILS
NOT TO SCALE

PROJECT NAME: TOWN OF SWEDEN PROPOSED FIRE STATION SWEDEN INTERIOR ELEVATIONS

PROJECT # 19080 DRAWING # 19080

DESIGNED BY: Designer: Author: CHECKED BY: Checker: APPROVED BY: Approver: SCALE: As indicated PLAN DATE: 12/14/22 DATE ISSUED: 12/14/22

CLIENT & OWNER: TOWN OF SWEDEN 147 BRIDGTON ROAD SWEDEN, MAINE 04041

DRWN APPD:

REVISIONS:

NO. DATE DESCRIPTION

19080

19080

As indicated

12/14/22

12/14/22

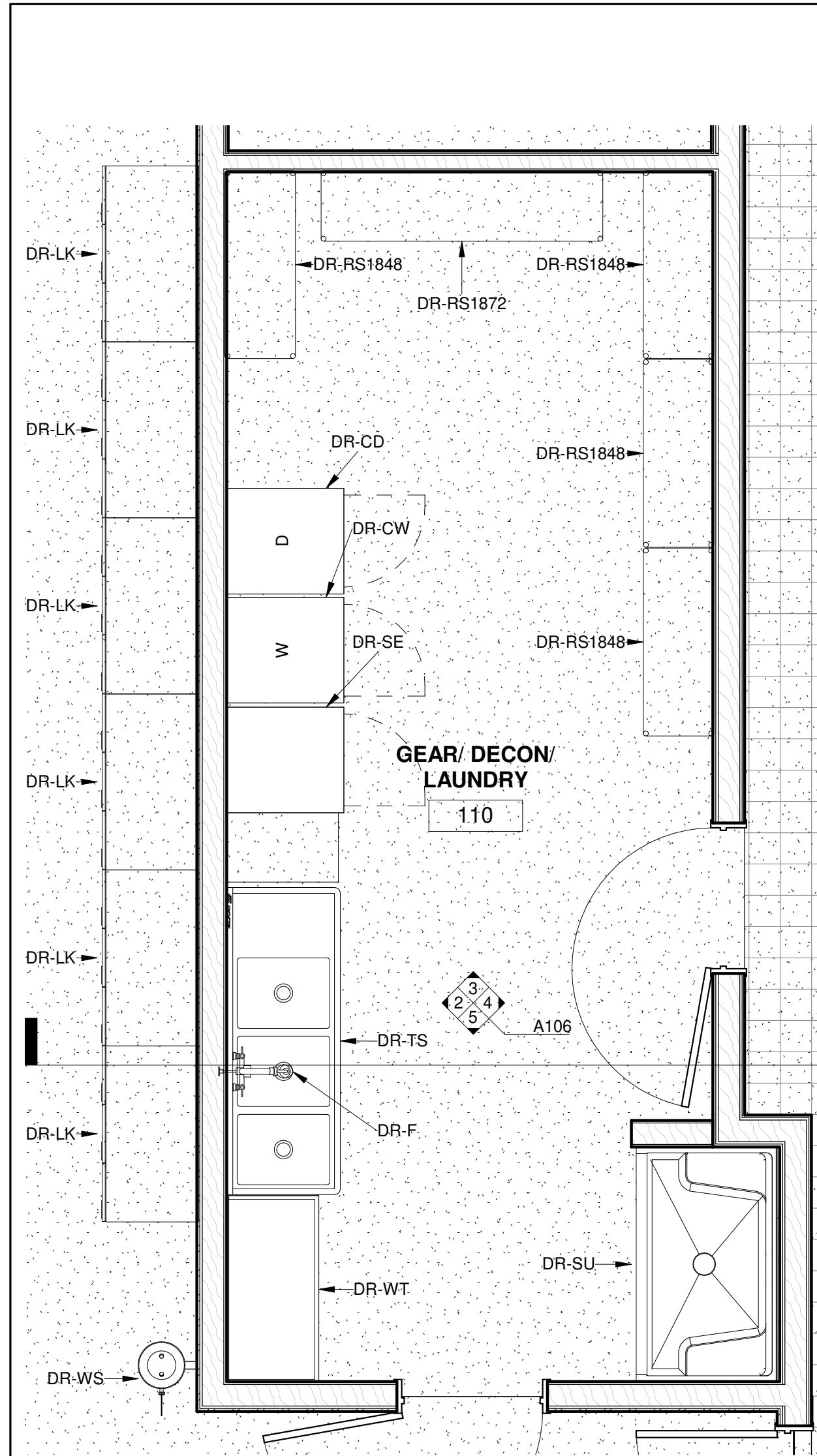
TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

STATE OF MAINE
KEITH C. GORHAM
REGISTERED PROFESSIONAL ENGINEER
LICENSE NO. 10123
DRAWINGS NOT SEALED ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

SHEET A105 OF 16

A105

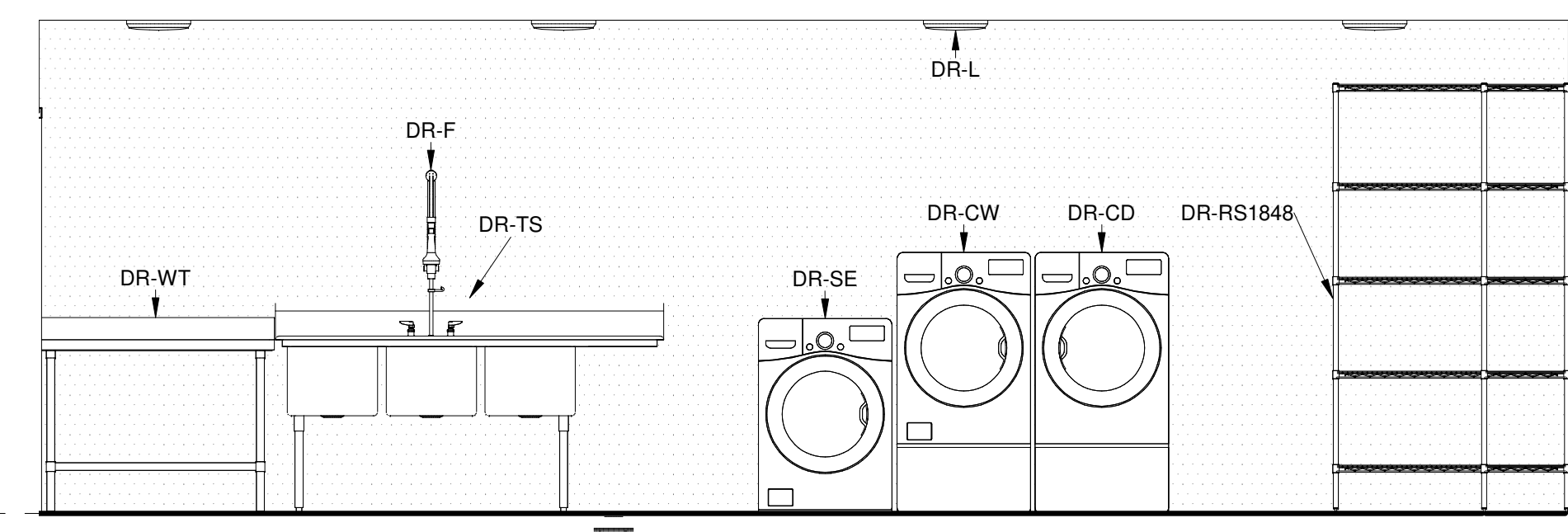


1 FIRST FLOOR
100' - 0"

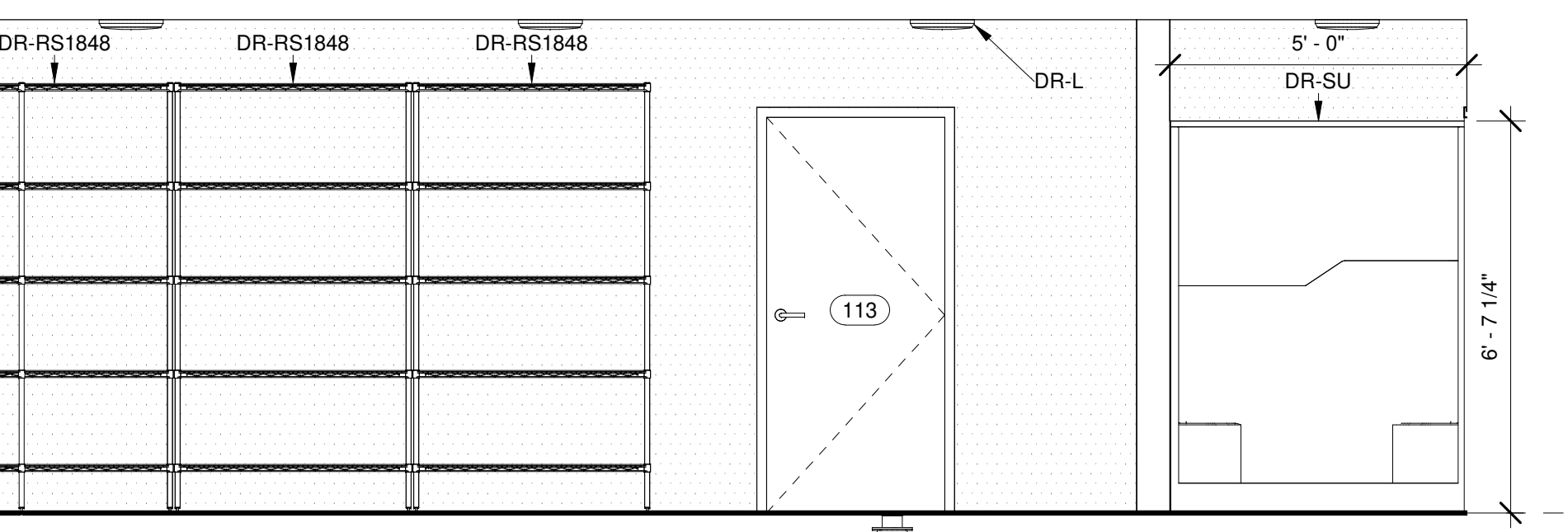
2 RM. 110-W
3/8" = 1'-0"

3 FIRST FLOOR
100' - 0"

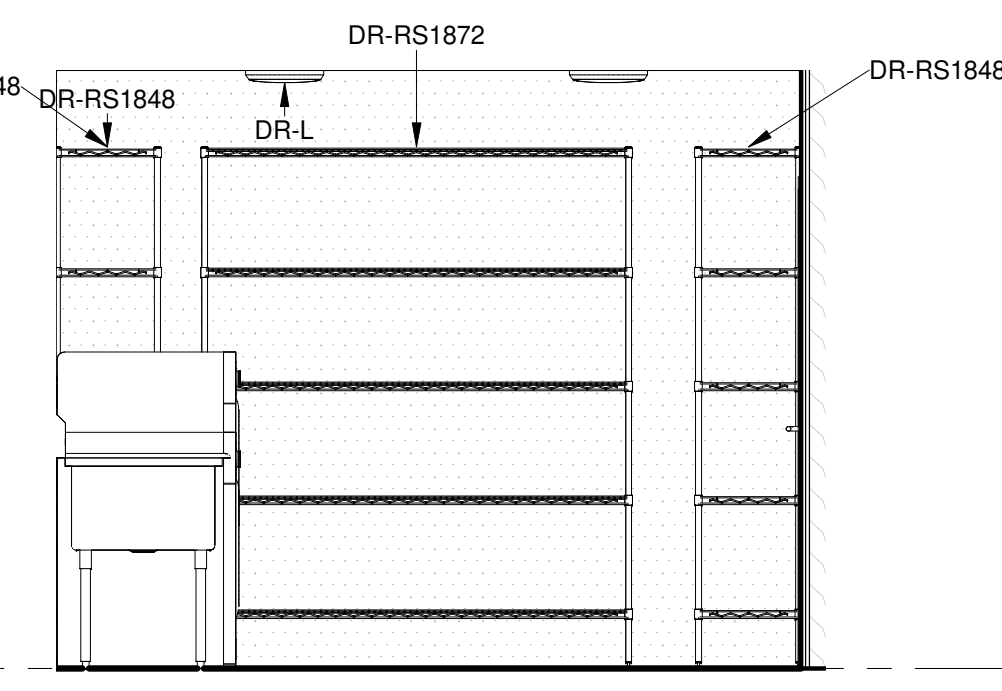
4 RM. 110-E
3/8" = 1'-0"



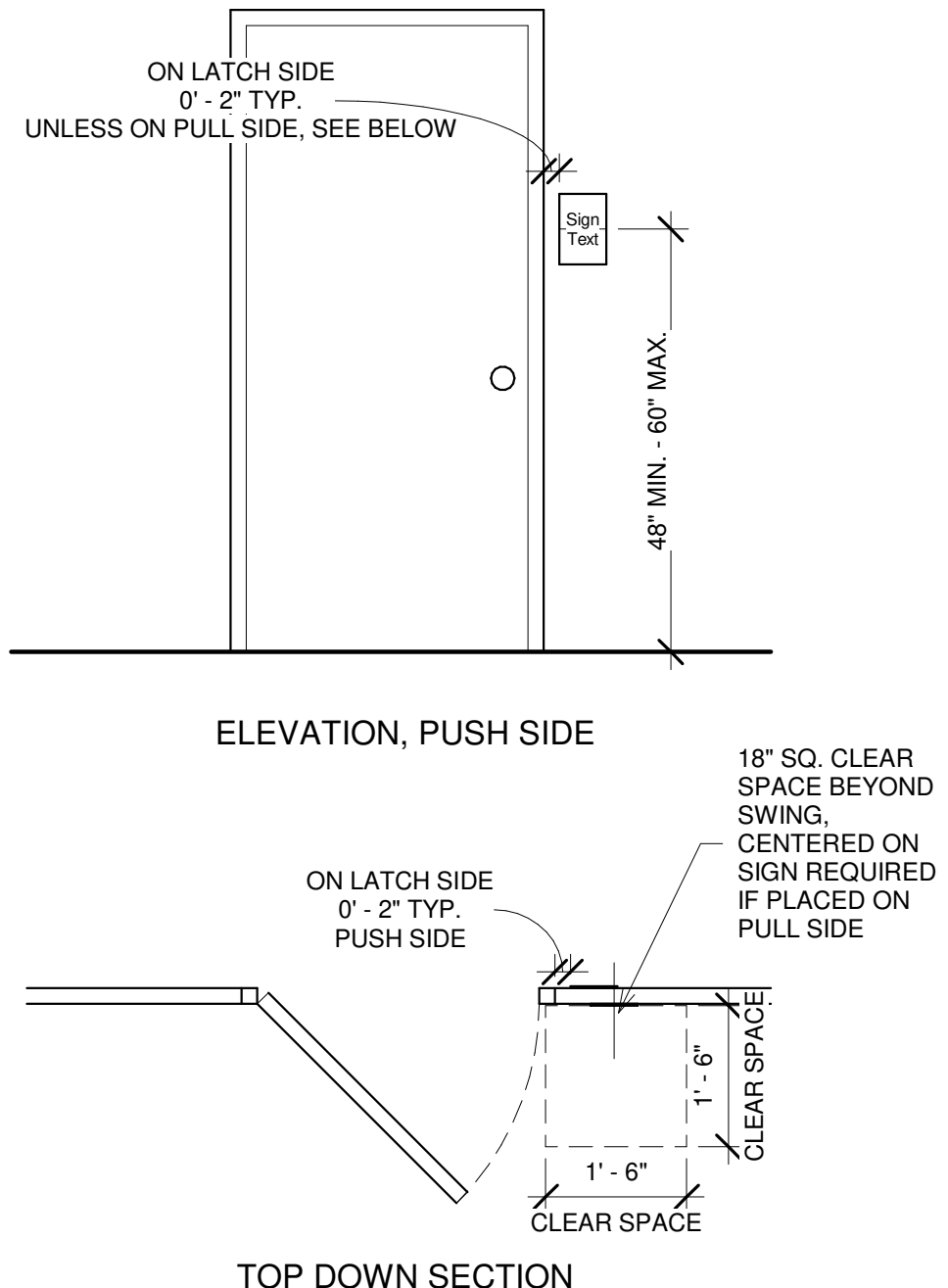
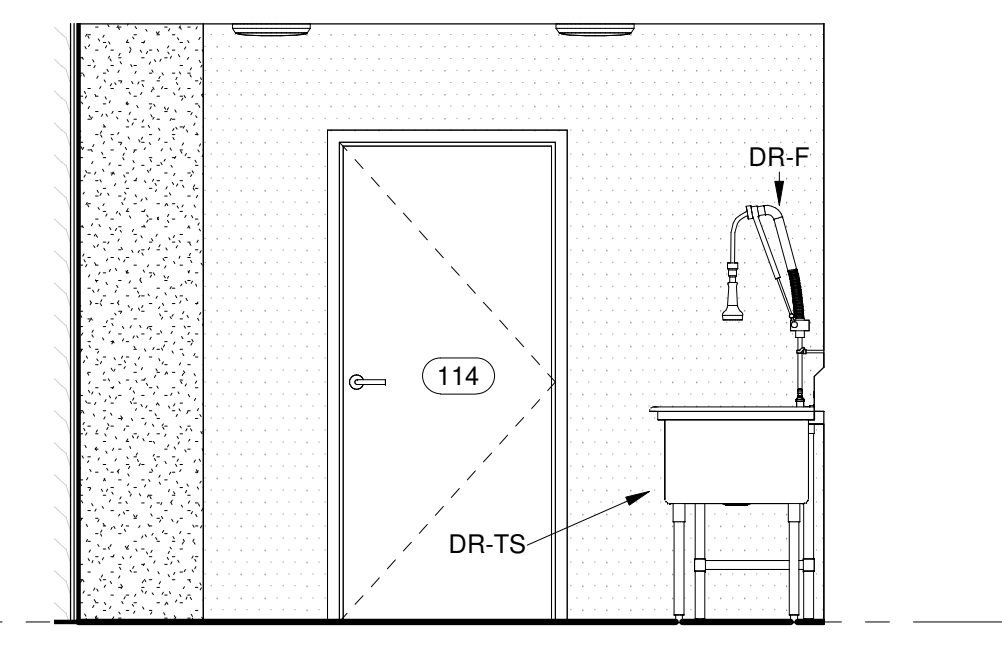
3 FIRST FLOOR
100' - 0"



5 RM. 110-S
3/8" = 1'-0"



3 FIRST FLOOR
100' - 0"



6 ADA SIGN LOCATIONS
1/2" = 1'-0"

1 FIRST FLOOR - 109-110
3/8" = 1'-0"

DECON ROOM 110 FIXTURES AND ACCESSORIES			
MARK	DESCRIPTION	CONTRACTOR INSTALLED	CONTRACTOR SUPPLIED, INSTALLED
DR-CD	CLOTHES DRIER, 30" FRONT LOAD, PEDESTAL, SUPPLIED BY OWNER	YES	NO
DR-CW	CLOTHES WASHER, 30" FRONT LOAD, PEDESTAL, SUPPLIED BY OWNER	YES	NO
DR-F	FAUCET, SPRING GOOSE NECK, WALL MOUNT	YES	YES
DR-L	LED LIGHT - SEE REFLECTIVE CEILING PLAN	YES	YES
DR-LK	20"x20"x72" STEEL MESH TURNOUT GEAR LOCKERS W/ HELMET AND BOOT SHELVES	YES	NO
DR-RS1848	WIRE RACK SHELF, CHROME, HD, 5 SHELVES, 18"x48"x72"	YES	NO
DR-RS1872	WIRE RACK SHELF, CHROME, HD, 5 SHELVES, 18"x72"x72"	YES	NO
DR-SE	CLOTHES SMOKE EXTRACTOR UNIT, SUPPLIED BY OWNER	YES	NO
DR-SU	SHOWER UNIT, FIBERGLASS SURROUND 5" WIDE, W/ CURTAIN	YES	YES
DR-TS	SINK, 3 BAY DEEP TUB, SS, RT. EXTENSION, W/ DRAINS	YES	YES
DR-WS	EMERGENCY EYE WASH STATION	YES	YES
DR-WT	WORK TABLE, 24"x60", SS, MARINE EDGE, BACKSPASH	YES	YES

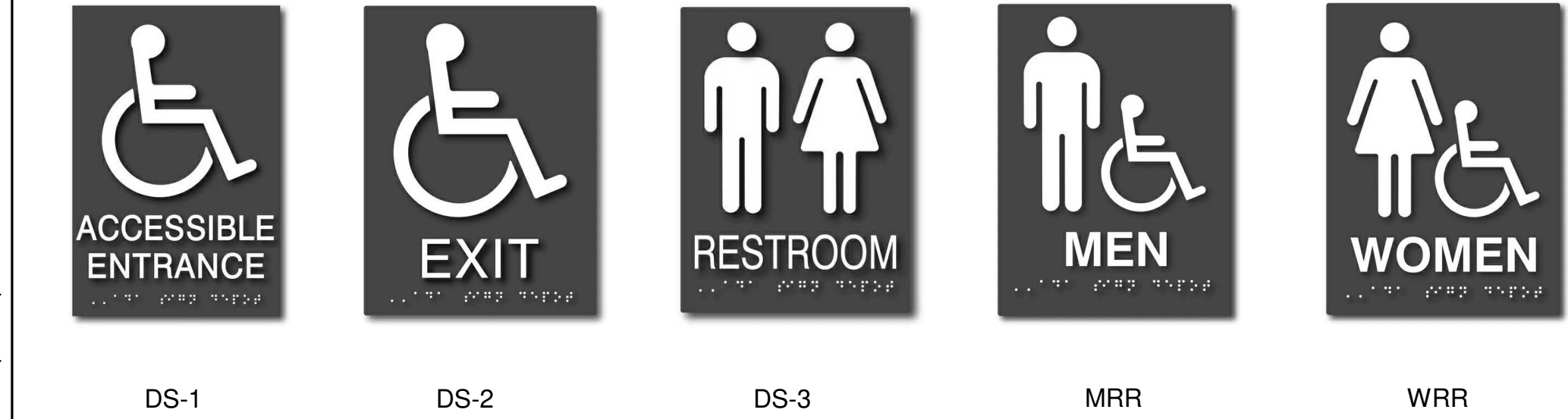
- ADA SIGN SPECIFICATIONS:**
- 6" WIDE x 9" TALL x 1/8" THK. HIGH IMPACT STYRENE PLASTIC, WHITE GRAPHICS.
 - BACKGROUND COLOR BLUE, WHITE GRAPHICS.
 - 1/2" MIN. ROUNDED CORNERS.
 - TACTILE GRAPHICS RAISED 1/32" MIN.
 - 5/8" HIGH, SIMPLE BLOCK FONT LETTERING.
 - GRADE 2 DOME-SHAPED BRAILLE.
 - MOUNT W/ FOAM STRIPS ALL-AROUND.
- SIGNAGE NOTES:**
- SUPPLY INTERIOR BRAIL SIGNAGE TO MEET ADA STANDARDS.
 - SUPPLY DIRECTION SIGNAGE TO BE DETERMINED IN FIELD.

DOOR SCHEDULE													
MARK	LOCATION	DOOR WIDTH	OPENING WIDTH	DOOR HEIGHT	OPENING HEIGHT	DOOR MATERIAL	TYPE	LITE KIT	FRAME MATERIAL	RATING	HARDWARE	COMMENTS	ADA SIGNAGE
101	HALL 102 END EXIT	3'-0"		7'-0"		MTL	SWING	3/4 LITE	MTL		C - T - H - L5 - KP - WS	INSULATED	DS-1 EXT. DS-2 INT.
102	APPARATUS BAY 101 SIDE EXIT	3'-0"		7'-0"		MTL	SWING		MTL		C - T - H - L4 - KP - WS	INSULATED	
103	HALL 102 SIDE EXIT	3'-0"		7'-0"		MTL	SWING	3/4 LITE	MTL		C - T - H - L5 - KP - WS	INSULATED	DS-1 EXT. DS-2 INT.
104	GARAGE 101 OHD	0'-0"	14'-0"	0'-0"	14'-0"	MTL	PANEL OVERHEAD TRACK	(4)12x24	MTL		OHD HARDWARE - PAD SWITCH - HD ELEC. CLOSER W/ 2 REMOTES	INSULATED	
105	GARAGE 101 OHD	0'-0"	14'-0"	0'-0"	14'-0"	MTL	PANEL OVERHEAD TRACK	(4)12x24	MTL		OHD HARDWARE - PAD SWITCH - HD ELEC. CLOSER W/ 2 REMOTES	INSULATED	
106	GARAGE 101 OHD	0'-0"	14'-0"	0'-0"	14'-0"	MTL	PANEL OVERHEAD TRACK	(4)12x24	MTL		OHD HARDWARE - PAD SWITCH - HD ELEC. CLOSER W/ 2 REMOTES	INSULATED	
107	MECH. 103	3'-0"		6'-8"		MTL	SWING		MTL	3/4 HR.	C - H - L4 - KP - SM		RN
108	APPARATUS BAY 101 / HALL 102	3'-0"		6'-8"		MTL	SWING		MTL	1-1/2 HR.	C - H - L3 - KP - SM		RN
109	APPARATUS BAY 101 / HALL 102	3'-0"		6'-8"		MTL	SWING	4x24 MAX.	MTL	1-1/2 HR.	C - H - L3 - KP - SM		RN
110	CHIEFS OFFICE 105	3'-0"		6'-8"		WD	SWING	6x28	MTL		H - L4 - DS - KP		RN
111	WRR 106	3'-0"		6'-8"		WD	SWING		MTL		H - L2 - DS - KP		WRR
112	MRR 107	3'-0"		6'-8"		WD	SWING		MTL		H - L2 - DS - KP		MRR
113	DECON 110 / HALL 102	3'-0"		6'-8"		MTL	SWING		MTL	1-1/2 HR.	C - H - L3 - KP - SM		RN
114	DECON 110 / APPARATUS BAY 101	3'-0"		6'-8"		MTL	SWING	4x24 MAX.	MTL	3/4 HR.	C - H - L3 - KP - SM	170 DEGREE SWING CLOSER REQUIRED	
115	CASCADE 109	3'-0"		6'-8"		MTL	SWING	4x24 MAX.	MTL	3/4 HR.	C - H - L3 - KP - SM		



- FINISH NOTES:**
- ALL GWB TO BE PAINTED 1 COAT PRIMER & 2 COATS FINISH.
 - FINISH OF CLOSETS SHOULD MATCH FINISH OF ROOM THE CLOSET IS INSTALLED IN UNLESS SPECIFICALLY NOTED IN THE TABLE OTHERWISE.
- FLOORING NOTES:**
- ALL INTERIOR FLOOR FINISHES OTHER THAN CARPETING SHALL MEET FLAME RETARDENT REQUIREMENTS AS SPECIFIED UNDER NFPA 101-10.2, CLASS II RATING.

- DOOR SCHEDULE ABBREVIATION KEY**
- C - CLOSER LCN ADA COMPLIANT
 - DS - DOOR STOP: GYLYNN-JOHNSON FLOOR MOUNTED TYPE FB 19
 - EH - EXIT HARDWARE SARGENT
 - H - HINGES, 1 1/2" PAIR, 4 1/2"x4 1/2", S.S. BALL BEARING NON-REMOVABLE PINS ON EXTERIOR, FULL MORTISE
 - KP - KICK PLATE: HAGAR, 14" HIGH, SS FINISH
 - T - ALUMINUM THRESHOLD, 6" W x 1/2" H, LENGTH TO FIT DOOR OPENING
 - L1 - LOCKSET: STORAGE FUNCTION, CYLINDER LOCK WITH LEVER HANDLE
 - L2 - LOCKSET: PRIVACY FUNCTION, CYLINDER LOCK WITH LEVER HANDLE
 - L3 - LOCKSET: PASSAGE FUNCTION, CYLINDER LOCK WITH LEVER HANDLE
 - L4 - LOCKSET: CLASSROOM FUNCTION, CYLINDER LOCK WITH LEVER HANDLE
 - L5 - LOCKSET: PROGRAM PUSH-BUTTON, CYLINDER LOCK WITH LEVER HANDLE
 - L6 - LOCKSET: EXTERIOR CARD READER, CYLINDER LOCK WITH LEVER HANDLE
 - PK - POCKET DOOR HARDWARE
 - PP - PUSH PLATE
 - DP - DOOR PULL
 - DS - DOOR STOP
 - PH - PEEPHOLE DOOR VIEWER, WIDE ANGLE, FLUSH
 - SM - SMOKE SEAL
 - WS - WEATHER STRIP: ZERO TYPE 312 AT JAMBS & HEAD
 - AC - AUTOMATIC DOOR OPENER W/ HC PAD OPERATORS
 - FB - FLUSH BOLT
 - DB - DEAD BOLT
 - RC - REMOTELY OPERATED ELEC. SECURITY LOCK



Legend - ADA SIGNAGE
3" = 1'-0"

WINDOW SCHEDULE							
TYPE MARK	WIDTH	HEIGHT	SILL HEIGHT	MANUFACTURER	Designation	FAMILY	COMMENTS
A	2' - 11 5/8"	4' - 8 7/8"	2' - 8"	Andersen Corporation	TW21046	Window-Double_Hung-Andersen-400_Series-Tilt_Wash	Vinyl-clad hung window

Grand total: 4

REVISIONS

NO.	DATE	DESCRIPTION

PROJECT NAME: TOWN OF SWEDEN
DRAWING # 19080
DESIGNED BY: Designer
DRAWN BY: Author
CHECKED BY: Checker
APPROVED BY: Approver
DATE: 12/14/22
SCALE: As Indicated
DATE ISSUED: 12/14/22

TOWN OF SWEDEN
147 BRIDGTON ROAD
SWEDEN, MAINE 04041

PROPOSED FIRE STATION
SWEDEN
SHEET NAME: INTERIOR ELEVATIONS, SCHEDULES

19080
19080
12/14/22

Plymouth Engineering, Inc.
8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

STATE OF MAINE
LICENSED PROFESSIONAL ENGINEER
KEITH C. SWANSON
LICENSE # 10000
DRAWINGS NOT SEALED ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

SHEET A106 OF 16

A106

Town of Sweden, Maine Comprehensive Plan 2004

Section II J Public Facilities and Services

1. Implementation Strategies

5. Commence detailed planning, make grant applications, and appropriate funds for the construction, in the designated Growth Area, of a new OSHA-approved fire station, if feasible, and maintain the standards required for continual mutual aid with neighboring communities.

Responsibility: Selectmen, Fire Department

Time Frame: Short-term

WARRANT FOR TOWN MEETING

To Frances V. Flint, Constable of the Town of Sweden, County of Oxford, State of Maine,

GREETINGS: In the name of the State of Maine you are hereby required to notify and warn the inhabitants of the Town of Sweden in said county, qualified by law to vote in Town affairs, to meet at Sweden Town Meeting House, in said Town, on Saturday, the 18th day of March AD 2006, at nine o'clock in the morning, then and there to act on the following Articles, to wit:

Article 1. To choose a Moderator to preside at said meeting.

Article 2. To see if the Town will vote to authorize the Board of Selectmen to borrow monies in anticipation of taxes as may be required to operate the services of the Town.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 3. To elect all Town Officers: (1) Selectman, Assessor and Overseer of the Poor for a period of three years; (1) Planning Board member; (2) Planning Board alternates; (1) Appeals Board member, 5 years; (2) Appeals Board alternates; (1) S.A.D. member, 3 years; (1) S.A.D. #72 alternate, 1 year; and any other necessary officers.

Article 4. To see if the town will vote to increase the property tax levy limit of \$184,041.00 established for the Town of Sweden by State law in the event that the municipal budget approved under the following articles will result in a tax commitment that is greater than that property tax levy limit.

Selectmen recommend (3-0)
Budget Committee

Article 5a. To see what sum the town will vote for the compensation of the Town Treasurer.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Treasurer, Tax Collector and Excise Tax Collector	\$6,388.06	\$6,739.40	
(Included in Town Charges)			
Selectmen recommend (1-0)			\$16,900.00
Budget Committee (5-0)			\$7,488.00

Article 5b. To see what sum the town will vote for the compensation of the Town Clerk.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Town Clerk	\$1,829.28	\$1,929.89	
(Included in Town Charges)			
Selectmen recommend (2-0)			\$16,900.00
Budget Committee (5-0)			\$5,200.00

Article 5c. To see what sum the town will vote for the compensation of the 1st, 2nd and 3rd Selectmen.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
1st, 2nd and 3rd Selectman	\$4,133.39	\$4,133.39	
(Included in Town Charges)			
Selectmen recommend (2-0)			\$4,340.00
Budget Committee (5-0)			\$4,340.00

Article 5d. To see what sum the town will vote for the compensation of the Fire Chief.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Fire Chief	\$1,200.00	\$1,250.00	
(Included in Sweden Vol. Fire Dept.)			
Selectmen recommend (2-0)			\$1,500.00
Budget Committee (5-0)			\$1,312.00

Article 5e. To see what sum the town will vote for the compensation of the Road Commissioner.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Road Commissioner	\$750.00	\$1,000.00	
(Included in Town Charges)			
Selectmen recommend (2-0)			\$1,050.00
Budget Committee (5-0)			\$1,050.00

Article 5f. To see what sum the town will vote for the compensation of the Code Enforcement Officer.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Code Enforcement Officer	\$2,012.62	\$2,500.00	
(Included in Planning Board)			
Selectmen recommend (2-0)			\$2,575.00
Budget Committee (5-0)			\$2,625.00

Article 5g. To see what sum the town will vote for the compensation of the Cemetery Sexton.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Town Cemetery Sexton	\$500.00	\$500.00	
(Included in Cemetery Account)			
Selectmen recommend (2-0)			\$250.00
Budget Committee (5-0)			\$250.00

Article 5h. To see what sum the town will vote for the compensation of the Administrative Assistant.

	<u>2004</u>	<u>2005</u>	<u>2006</u>
Town Administrative Assistant	\$14,400.00	\$14,850.00	
(Included in Town Charges)			
Selectmen recommend (2-0)			\$15,912.00
Budget Committee (5-0)			\$15,592.00

Article 6. To see if the Town will vote to take from surplus up to \$1,200.00, for payment of interest on tax anticipation borrowing for the ensuing year.

Selectmen recommend (2-0)		\$1,200.00
Budget Committee (5-0)		\$1,200.00

Article 7. To see what sum the town will raise or appropriate for Town Charges for the ensuing year.

Selectmen recommend (2-0)		\$95,000.00
Budget Committee (5-0)		\$73,568.00

Article 8. To see what sum the town will raise or appropriate for the maintenance of road and road sides for the ensuing year.

Selectmen recommend (1-0)		\$62,000.00
Budget Committee (5-0)		\$33,000.00

Article 9. To see what sum the town will raise or appropriate for snow removal and sanding for the ensuing year.

Selectmen recommend	(2-0)	\$118,745.00
Budget Committee	(5-0)	\$100,000.00

Article 10. To see what sum the town will raise or appropriate for rubbish removal/dump operation and to pay for any additional usage charges from surplus monies.

Selectmen recommend	(2-0)	\$39,000.00
Budget Committee	(5-0)	\$39,000.00

Article 11. To see what sum the Town will raise or appropriate for the Town's Spring/Fall pick up program and to pay for any additional usage charges from surplus monies.

Selectmen recommend	(2-0)	\$3,000.00
Budget Committee	(5-0)	\$1,500.00

Article 12. To see if the Town will vote to authorize the Selectmen/Assessors to draw upon the Surplus Account not more than .0003 of the present State Valuation of the Town, \$47,550,000 for the care of the poor for the ensuing year.

Selectmen recommend	(2-0)
Budget Committee	(5-0)

Article 13. To see what sum the town will raise or appropriate for grounds maintenance, upkeep and repairs for town buildings and to allow the balance to carry over each year.

Selectmen recommend	(2-0)	\$700.00
Budget Committee	(5-0)	\$700.00 no carryover

Article 14. To see what sum the town will raise or appropriate for support of the Sweden Volunteer Fire Department Association.

Selectmen recommend	(2-0)	\$25,904.00
Budget Committee recommends	(5-0)	\$25,716.00

Article 15. To see what sum the town will raise or appropriate to fund a Sweden Fire Department Building Committee.

Selectmen recommend	(2-0)	\$500.00
Budget Committee	(5-0)	\$500.00

Article 16. To see what sum the town will raise or appropriate for the administration of Emergency Management.

Selectmen recommend	(2-0)	\$500.00
Budget Committee	(5-0)	\$500.00

Article 17. To see what sum the town will authorize to take from surplus for the purpose of Purchasing Land for the Construction of a new Fire Station, and allow the balance to carryover.

Selectmen recommend	(3-0)	\$16,500.00
Budget Committee	No Recommendation	

Article 18. To see if the town will raise or appropriate \$2,400.00 for the support of the Ambulance Service for the ensuing year.

Selectmen recommend	(2-0)	\$2,400.00
Budget Committee	(5-0)	\$2,400.00

Article 19. To see what sum the town will raise or appropriate for the support of the Lovell recreation programs.

Selectmen recommend	(1-1)	\$600.00
Budget Committee	(5-0)	\$600.00

Article 20. To see what sum the town will raise or appropriate for the support of Charlotte E. Hobbs Memorial Library.

Selectmen recommend	(1-1)	\$1,500.00
Budget Committee	(5-0)	\$800.00

Article 21. To see what sum the town will raise or appropriate for the support of Sweden Conservation Committee.

Selectmen recommend	(2-0)	\$375.00
Budget Committee	(5-0)	\$375.00

Article 22. To see what sum the town will raise or appropriate for the support of Harvest Hills Animal Shelter.

Selectmen recommend	(2-0)	\$350.00
Budget Committee	(5-0)	\$350.00

Article 23. To see what sum the town will vote to raise or appropriate for the support of the Lakes Environmental Association for the ensuing year.

Selectmen recommend	(1-1)	\$400.00
Budget Committee	(4-0)	\$400.00

Article 24. To see what sum the town will vote to raise or appropriate for the support of the Community Concepts for the ensuing year.

Selectmen recommend	(2-0)	\$730.00
Budget Committee	(5-0)	\$ 0.00

Article 25. To see what sum the Town will vote to raise or appropriate for the support of Seniors Plus.

Selectmen recommend	(1-1)	\$100.00
Budget Committee	(5-0)	\$ 0.00

Article 26. To see what sum the town will raise or appropriate for Cemetery maintenance.

Selectmen recommend	(2-0)	\$3,000.00
Budget Committee	(5-0)	\$2,000.00

Article 27. To see if the Town will vote to authorize the Selectmen/Assessors to use not more than \$38,000.00 of the 2006 anticipated Excise monies collected in 2006 towards the amount appropriated in Article 5 through 26.

Selectmen recommend	(2-0)
Budget Committee	(5-0)

Article 28. To see if the Town will vote to authorize the Selectmen/Assessors to use not more than \$19,000.00 of the 2006 anticipated Tree Growth Refund monies collected in 2006 towards the amount appropriated in Article 5 through 26.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 29. To see if the voters will authorize the raising of the balance so appropriated, not covered in Article 27 and 28 through taxation.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 30. To see if the Town will vote to take not more than \$600.00 from the Snowmobile account for the purpose of maintaining public snowmobile and cross-country ski trails within the Town of Sweden, and allow the Selectmen to spend for that purpose.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 31. To see if the Town will vote to accept the highest interest rate set by the State of Maine Planning Office for delinquent taxes committed in the current year. (Rate cannot be greater than the State rate of 11% for 2006.)

Selectmen recommend (3-0)
Budget Committee (5-0)

Article 32. To see if the Town will vote to determine the date from and after which interest shall accrue on delinquent taxes committed for the current year.

Selectmen recommend (2-0) September 15, 2006
Budget Committee (5-0) September 15, 2006

Article 33. To see if the Town will vote to set the interest rate paid on abated taxes to 4 percentage points less than the rate used in Article 31 above by the State of Maine Planning Office; (can not be less than 4% of State rate on delinquent taxes committed in the current year).

Selectmen recommend (3-0)
Budget Committee (5-0)

Article 34. To see if the Town will authorize the Selectmen on behalf of the Town to sell and dispose of any real estate acquired by the Town for the non-payment of taxes thereon; said real estate to be advertised in two separate issues of local paper within 30 days of sale. To be sold either at public auction or as the result of sealed bids called for and received by the Selectmen; provided, however, that the selectmen be authorized to sell at private sale property so acquired to the owner on record; of any such property; all conveyances there-under to be by release deed.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 35. To see if the Town will vote to authorize the municipal officers to spend an amount not to exceed 3/12 of the budgeted amount in each budget category of the 2006 annual budget during the period of January 1, 2007 to the March Annual Town Meeting in 2007.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 36. To see if the Town will vote to take from surplus up to \$10,000.00 for the purpose of funding capital projects on State Aid roads and allow the balance to carry over each year.

Selectmen recommend (2-0)
Budget Committee (5-0)

Article 37. To see what sum the Town will vote to take from surplus for the purpose of re-evaluating the town's assessment and to allow the balance to carry over.

Selectmen recommend (2-0) \$19,750.00
Budget Committee (5-0) \$19,750.00

Article 38. To see if the Town will vote to take from surplus \$2,500.00 to replenish the contingency fund and allow the balance to carry over each year.

Selectmen recommend (2-0) \$2,500.00
Budget Committee No Recommendation

Article 39. To see if the Town to authorize the Assessors to take from the Town's un-appropriated fund balance ("town surplus") any amount up to \$37,584.00 and apply it to reduce the tax commitment for the town's Fiscal Year 2006.

Information note: Due to an erroneous accounting of school funds needed for fiscal year 2005, the funds raised by 2005 town meeting exceeded the Town's actual budget needs by \$37,584.00. The taxes committed and collected for 2005 therefore resulted in a budget surplus of \$37,584.00. This article seeks to apply these surplus funds toward reducing the amount of total tax commitment for fiscal year 2006.

Selectmen (3-0)
Budget Committee

Given under our hands this 28 February 2006
Selectmen [Signature]
of [Signature]
Sweden [Signature]
ATTEST: [Signature]
Frances V. Flint

Constables Return
Town of Sweden

Oxford, SS.

Pursuant to the within warrant to me directed, I have notified and warned the inhabitants of said Town, qualified as therein expressed, to meet at said time and place and for the purposes therein names by posting a copy of said warrant attested by me at the Town Hall a public and conspicuous place on 28 February 2006 being at least seven days before the meeting.

Frances V. Flint, Constable

Minutes for Town Meeting

To William Graham, Constable of the Town of Sweden, County of Oxford, State of Maine,

GREETINGS: In the name of the State of Maine you are hereby required to notify and warn the inhabitants of the Town of Sweden in said county, qualified by law to vote in Town affairs, to meet at the Barn at the Old Saco Inn, 125 Old Saco Lane, Fryeburg, Maine, **on Saturday, the 27th day of March AD 2021, at nine o'clock in the morning**, then and there to act on the following Articles, to wit:

Meeting was called to order at 9am by Clerk Patricia Porter and opened with the Pledge of Allegiance.

Article 1. To choose a Moderator to preside at said meeting.

***Nomination of Walter Riseman by Alberta Ridlon
Second by Patrick Carty
Walter Riseman took Oath***

Article 2. To see if the Town will authorize the Board of Selectmen to borrow monies in anticipation of taxes as may be required to operate the services of the Town.

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

***Motion to accept by Alberta Ridlon
Second by Patrick Carty
Motion Accepted***

Article 3. To elect all Town Officers: (1) Selectmen, Assessor and Overseer of the Poor for a period of 3 years; (1) Selectmen, Assessor and Overseer of the Poor for a period of 2 years ; (1) Planning Board member for a period of 5 years; (2) Planning Board alternates each for a period of 1 year; (1) Appeals Board members for a period of 5 years; (1) Appeals Board member for a period of 4 years; (1) Appeals Board member for a period of 1 year; (2) Appeals Board alternates each for a period of 1 year; (1) SAD #72 Board Member alternate for a period of 3 years; and any other necessary officers.

***Selectperson 3 year
Alberta Ridlon nominated by Melanie Eldracher
Second by Barbara Graham
No other nominations
Alberta Ridlon was elected by written vote.***

***Selectperson 2 year
Warren deWildt nominated by Bill Graham
Second by Howard Buker
No other nominations
Warren deWildt was elected by written vote***

***Planning Board Member 5 year
Dave Johnson nominated by Bill Graham
Second by Lynn Hopkins***

No other nominations
Dave Johnson elected by show of hands

Planning Board Alternate 1 year
Lynn Hopkins nominated by Laura Chadbourne
Second by Melanie Eldracher
No other nominations
Lynn Hopkins elected by show of hands

Planning Board Alternate 1 year
Jane Gibbons nominated by David Johnson
Second by Kay Lyman
Bruce Taylor nominated by Lynn Hopkins
Second Bill Graham
Two nominations and vote by show of hands
Bruce Taylor elected by show of hands

Appeals Board 4 year
Dell Foss nominated by Lynn Hopkins
Second by Julie McQueen
No other nominations
Dell Foss elected by show of hands

Appeals Board 5 years
Jane Gibbons nominated by Kay Lyman
Second by Lynn Hopkins
No other nominations
Jane Gibbons elected by show of hands

Appeals Board 1 year
Perri Black nominated by Kay Lyman
Second by Jane Gibbons
No other nominations
Perri Black elected by show of hands

Appeals Board alternate
No nominations

Appeals Board 3 years
Marilyn Smith nominated by Dell Foss
Second by Cheryl Bedan
No other nominations
Marilyn Smith elected by show of hands

Article 4. To see if the town will increase the property tax levy limit established for the Town of Sweden by State law in the event that the municipal budget approved under the following articles will result in a tax commitment that is greater than that property tax levy limit.

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne

Second by Patrick Carty

Article approved by show of hands

Article 5a. To see what sum the town will raise and appropriate for the compensation of the 1st, 2nd and 3rd

<u>Selectmen.</u>	<u>2019</u>	<u>2020</u>	
	\$8,844.00	\$9,021.00	
Selectmen recommend raise (3-0)			\$9,245.00
Budget Committee recommends raise (3-0)			\$9,245.00

Motion to accept as written by Chris Chadbourne

Second by Dell Foss

Article approved by show of hands

Article 5b. To see what sum the town will raise and appropriate for the compensation of the Town Clerk.

	<u>2019</u>	<u>2020</u>	
	\$8,792.00	\$8,968.00	
Selectmen recommend raise (3-0)			\$9,147.00
Budget Committee recommends raise (3-0)			\$9,147.00

Motion to accept as written by Chris Chadbourne

Second by Jane Gibbons

Article approved by show of hands

Article 5c. To see what sum the town will raise and appropriate for the compensation of the Town Treasurer.

Selectmen recommend raise (3-0)		\$4,771.00
Budget Committee recommends raise (3-0)		\$4,771.00

Motion to accept as written by Chris Chadbourne

Second by Dell Foss

Article approved by show of hands

Article 5d. To see what sum the town will raise and appropriate for the compensation of the Town Tax Collector.

	<u>2019</u>	<u>2020</u>	
	\$13,377.00	\$13,645.00	
Selectmen recommend raise (3-0)			\$9,147.00
Budget Committee recommends raise (3-0)			\$9,147.00

***The position of Treasurer/ Tax Collector will be split into 2 separate positions.**

Motion to accept as written by Chris Chadbourne
Second by Dell Foss
Article approved by show of hands

Article 5e. To see what sum the town will raise and appropriate for the compensation of the **Administrative Assistant to the Select Board at an average of 12 hours per week for 52 weeks per year.**

	<u>2019</u>	<u>2020</u>	
	\$26,873.60	\$17,160.00	
Selectmen recommend raise (3-0)			\$10,483.00
Budget Committee recommends raise (3-0)			\$10,483.00

Motion to accept as written by Chris Chadbourne
Second by Alberta Ridlon
Article approved by show of hands

Article 5f. To see what sum the town will raise and appropriate for the compensation of the **Code Enforcement Officer.**

	<u>2019</u>	<u>2020</u>	
	\$7,803.00	\$8,210.00	
Selectmen recommend raise (3-0)			\$8,119.00
Budget Committee recommends raise (3-0)			\$8,119.00

***The 2020 stipend included \$250 for E-911 addressing officer which is now a separate article.**

Motion by Chris Chadbourne to amend to \$12,000
Second by Patrick Carty
Article approved at \$12,000 by show of hands

Article 5g. To see what sum the town will raise and appropriate for the compensation of the **Fire Chief.**

	<u>2019</u>	<u>2020</u>	
	\$1,912.00	\$1,951.00	
Selectmen recommend raise (3-0)			\$1,990.00
Budget Committee recommends raise (3-0)			\$1,990.00

Motion to amend to \$2,990.00 by Gail deWildt
Second by Doug Porter
Vote taken and amendment failed
Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands at \$1,990.00

Article 5h. To see what sum the town will raise and appropriate for the compensation of the **Road Commissioner.**

	<u>2019</u>	<u>2020</u>	
	\$2,413.00	\$2,462.00	
Selectmen recommend raise (3-0)			\$2,511.00
Budget Committee recommends raise (3-0)			\$2,511.00

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 5i. To see what sum the town will raise and appropriate for the compensation of the **Cemetery Sexton.**

	<u>2019</u>	<u>2020</u>	
Selectmen recommend raise (3-0)	\$319.00	\$326.00	\$332.00
Budget Committee recommends raise (3-0)			\$332.00

***Motion to accept as written by Chris Chadbourne
Second by Alberta Ridlon
Article approved by show of hands***

Article 5j. To see what sum the town will raise and appropriate for the compensation of the **Emergency Management Director.**

	<u>2019</u>	<u>2020</u>	
Selectmen recommend raise (3-0)	\$1,230.00	\$1,255.00	\$500.00
Budget Committee recommends raise (3-0)			\$500.00

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 5k. To see what sum the town will raise and appropriate for the compensation of the **E-911 Addressing Officer.**

Selectmen recommend raise (3-0)		\$250.00
Budget Committee recommends raise (3-0)		\$250.00

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 5l. To see what sum the Town will raise and appropriate for the compensation of Town employees for the purpose of training newly hired Town employees. Compensation will be paid at \$16.50 per hour. FICA and Medicare to come out of Town Charges.

Selectmen recommend raise (3-0)		\$2,500.00
Budget Committee recommends raise (3-0)		\$2,500.00

***Motion to accept as written by Chris Chadbourne
Second by Melanie Eldracher
Article approved by show of hands***

Article 6. To see if the Town will take from surplus up to \$1,200.00 for payment of interest on tax anticipation borrowing for the ensuing year.

Selectmen recommend take from surplus (3-0)		up to \$ 1,200.00
Budget Committee recommends take from surplus (3-0)		up to \$ 1,200.00

***Motion to accept as written by Chris Chadbourne
Second by Laura Chadbourne
Article approved by show of hands***

Article 7. To see what sum the town will raise and appropriate for **Town Charges** for the ensuing year.
Selectmen recommend raise (3-0) \$76,700.00
Budget Committee recommends raise (3-0) \$76,700.00

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 8. To see what sum the Town will raise and appropriate for support of the Sweden Planning Board.
Planning Board requested \$16,000.00
Selectmen recommend raise (3-0) \$16,000.00
Budget Committee recommends raise (3-0) \$16,000.00

***Motion to accept as written by Chris Chadbourne
Second by Lynn Hopkins
Article approved by show of hands***

Article 9. To see what sum the Town will vote to raise and appropriate and/or take from surplus for the purpose of revising **Comprehensive Plan** carry forward until the cost of the revision has been paid in full.
Selectmen recommend take from surplus (3-0) up to \$10,000.00
Budget Committee recommends take from surplus (3-0) up to \$10,000.00
*There is \$2,060 in a carry forward account from the last Comprehensive Plan.

***Motion to accept as written by Chris Chadbourne
Second by Melanie Eldracher
Article approved by show of hands***

Article 10. To see what sum the town will raise and appropriate for the **maintenance of roads and roadsides** for the ensuing year.
Selectmen recommend raise (3-0) \$101,000.00
Budget Committee recommends raise (3-0) \$101,000.00

***Motion to accept as written by Chris Chadbourne
Second by Alberta Ridlon
Article approved by show of hands***

Article 10a. To see what sum the Town will vote to raise and appropriate in order to establish a **Capital Improvement Reserve Account for Roads**, pursuant to 30-A M.R.S Section 5801, and allow the balance to carry forward.
Selectmen recommend raise (3-0) \$69,000.00
Budget Committee recommends raise (3-0) \$69,000.00

Motion to accept as written by Chris Chadbourne

Second by Patrick Carty

Article approved by show of hands

Article 11. To see what sum the town will raise and appropriate for the purpose of **snow removal and sanding** for the ensuing year.

Selectmen recommend raise (3-0)	\$247,000.00
Budget Committee recommends raise (3-0)	\$247,000.00

Motion to accept as written by Chris Chadbourne

Second by Alberta Ridlon

Article approved by show of hands

Article 12. To see what sum the town will raise and appropriate to pay for **Lovell dump contract** and to pay for any additional usage charges from surplus monies.

Selectmen recommend raise (3-0)	\$50,613.49
Budget Committee recommends raise (3-0)	\$50,613.49

Motion to accept as written by Chris Chadbourne

Second by Dell Foss

Article approved by show of hands

Article 13. To see what sum the Town will authorize the Selectmen to draw upon the surplus account for the care of the poor for the ensuing year.

Selectmen recommend (3-0)	up to \$1,250.00
Budget Committee recommends (3-0)	up to \$1,250.00

Motion to accept as written by Chris Chadbourne

Second by Patrick Carty

Article approved by show of hands

Article 14. To see what sum the town will raise and appropriate for **grounds maintenance, upkeep and repairs for town buildings**.

Selectmen recommend raise (3-0)	\$10,000.00
Budget Committee recommends raise (3-0)	\$10,000.00

Motion to accept as written by Chris Chadbourne

Second by Kay Lyman

Article approved by show of hands

Article 15. To see what sum the town will raise and appropriate for support of the **Sweden Volunteer Fire Department**.

SVFA requested	\$50,000.00
Selectmen recommend raise (3-0)	\$50,000.00
Budget Committee recommends raise (3-0)	\$50,000.00

***Motion to accept as written by Chris Chadbourne
Second by Lynn Hopkins
Article approved by show of hands***

Article 15a. To see what sum the town will take from surplus for the purpose of **Sweden Volunteer Fire Department capital improvements for fire apparatus** and allow the balance to carry forward

SVFA requested	\$10,000.00
Selectmen recommend take from surplus (3-0)	\$10,000.00
Budget Committee recommends take from surplus (3-0)	\$10,000.00

***Motion to accept as written by Chris Chadbourne
Second by Bruce Taylor
Article approved by show of hands***

Article 16. To see what sum the town will raise and appropriate for support of the **ambulance service** for the ensuing year.

Selectmen recommend raise (3-0)	\$2,040.00
Budget Committee recommends raise (3-0)	\$2,040.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 17 To see what sum the town will raise and appropriate for the support of **Harvest Hills Animal Shelter**.

Selectmen recommend raise (3-0)	\$400.00
Budget Committee recommends raise (3-0)	\$400.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 18. To see what sum the Town will raise and appropriate for the support of **Seniors Plus.**

Selectmen recommend raise (3-0)	\$500.00
Budget Committee recommends raise (3-0)	\$500.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 19. To see what sum the town will raise and appropriate for the support of **Sweden Conservation Committee.**

Selectmen recommend raise (3-0)	\$125.00
Budget Committee recommends raise (3-0)	\$125.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 20. To see if the Town will take from surplus up to \$2,400.00 for support of the **Sweden Historical Society (SHS)** for installation of a heat pump in the Center Schoolhouse. Funds not to be distributed until the SHS obtains a permit from the Office of the State Fire Marshall and a Conditional Use Permit from the Sweden Planning Board.

Selectmen recommend take from surplus (3-0)	up to \$2,400.00
Budget Committee recommends take from surplus (3-0)	up to \$2,400.00

***Motion to accept as written by Chris Chadbourne
Second by Jane Gibbons
Article approved by show of hands***

Article 21. To see what sum the town will raise and appropriate for the support of **Charlotte E. Hobbs Memorial Library.**

Selectmen recommend raise (2-0)	\$2,400.00
Budget Committee recommends raise (3-0)	\$2,400.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 22. To see what sum the town will raise and appropriate for the support of **Lakes Environmental Association (LEA).**

Selectmen recommend raise (3-0)	\$400.00
Budget Committee recommends raise (3-0)	\$400.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 23. To see what sum the town will raise and appropriate for the support of **Sweden Food Pantry**

Selectmen recommend raise (3-0)	\$600.00
Budget Committee recommends raise (3-0)	\$600.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 24. To see what sum the town will raise and appropriate for the support of Lovell Food Pantry

Selectmen recommend raise (3-0)	\$500.00
Budget Committee recommends raise (3-0)	\$500.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 25. To see what sum the town will raise and appropriate for the purpose of Community Concepts.

Selectmen recommend raise (3-0)	\$300.00
Budget Committee recommends raise (3-0)	\$300.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 26. To see what sum the town will raise and appropriate for the support of The LifeFlight Foundation.

Selectmen recommend raise (3-0)	\$391.00
Budget Committee recommends raise (3-0)	\$391.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 27. To see what sum the town will raise and appropriate for the support of Safe Voices.

Selectmen recommend raise (3-0)	\$300.00
Budget Committee recommends raise (3-0)	\$300.00

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 28. To see what sum the town will raise and appropriate for the purpose of supporting the new Sweden Communications Committee.

Selectmen recommend raise (3-0)	\$100.00
Budget Committee recommends raise (3-0)	\$100.00

***Motion to accept as written by Chris Chadbourne
Second by Alberta Ridlon
Article approved by show of hands***

Article 29. To see what sum the town will take from surplus for the purpose of Cemetery maintenance that is the responsibility of the Town.

Selectmen recommend take from surplus (3-0)	up to \$8,000.00
Budget Committee recommends take from surplus (3-0)	up to \$8,000.00

***Motion to amend to \$5,000.00 by Patrick Carty
Second Chris Chadbourne
Motion failed by show of hands
Motion to accept at \$8,000.00 by Jane Gibbons
Second by Alberta Ridlon
Article approved by show of hands at \$,8000.00***

Article 30. To see what sum the town will raise and appropriate and/or take from surplus for the purpose of legal counsel.

Selectmen recommend raise (3-0)	\$ 2,500.00
Selectmen recommends take from surplus if needed (3-0)	up to \$ 12,500.00
Budget Committee recommends raise (3-0)	\$ 2,500.00
Budget Committee recommends take from surplus if needed (3-0)	up to \$ 12,500.00

***Motion to accept as written by Chris Chadbourne
Second by Bruce Tayler
Article approved by show of hands***

Article 31. To see if the Town will authorize the Selectmen/Assessors to use not more than \$80,000.00 of the 2021 anticipated Excise monies collected in 2021 towards the amount appropriated in Articles 5a through 30.

Selectmen recommend (3-0)
Budget Committee recommends (3-0)

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 32. To see if the Town will authorize the Selectmen/Assessors to use not more than \$30,000.00 of the 2021 anticipated Tree Growth Refund monies collected in 2021 towards the amount appropriated in Articles 5a through 30.

Selectmen recommend (3-0)
Budget Committee recommends (3-0)

***Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands***

Article 33. To see if the voters will authorize the raising of the balance so appropriated, not covered in Article 31 and 32 through taxation.

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne

Second by Jane Gibbons

Article approved by show of hands

Article 34. To see if the Town will accept the highest interest rate set by the Office of the State Treasurer, State of Maine for delinquent taxes committed in the current year. (Rate cannot be greater than the State rate of 6% for 2021).

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne

Second by Kay Lyman

Article approved by show of hands

Article 35. To see if the Town will determine the date from and after which interest shall accrue on delinquent taxes committed for the current year.

Selectmen recommend (3-0)

60 days after the date of commitment.

Budget Committee recommends (3-0)

60 days after the date of commitment.

Motion to accept as written by Chris Chadbourne

Second by Lynn Hopkins

Article approved by show of hands

Article 36. To see if the Town will set the interest rate paid on abated taxes to 4 percentage points less than the rate used in Article 34 above by the State Treasurer, State of Maine.

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne

Second by Kay Lyman

Article approved by show of hands

Article 37. To see if the Town will authorize the Selectmen to take such sums from overlay as may be necessary for the purpose of funding tax abatements.

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

***Motion to accept as written by Chris Chadbourne
Second by Lynn Hopkins
Article approved by show of hands***

Article 38. To see if the Town will authorize the Selectmen on behalf of the Town to sell and dispose of any real estate acquired by the Town for the non-payment of taxes thereon; or by other judicial process; said real estate to be advertised in two separate issues of local paper within 30 days of sale. To be sold either at public auction or as the result of sealed bids called for and received by the Selectmen; provided, however, that the selectmen be authorized to sell at private sale property so acquired to the owner on record of any such property; all conveyances there-under to be by release deed. Except that the Municipal Officers shall use the special sale process required by 36 M.R.S. § 943-C for qualifying homestead property if they choose to sell it to anyone other than the former owner(s).

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

***Motion to accept as written by Chris Chadbourne
Second by Bruce Taylor
Article approved by show of hands***

Article 39. To see if the Town will authorize the municipal officers to spend an amount not to exceed 3/12 of the budgeted amount in each budget category of the 2021 annual budget during the period of January 1, 2022 to the March Annual Town Meeting in 2022. The exception is winter road maintenance. The municipal officers may spend more than 3/12ths of the 2021 annual budget amount for winter road maintenance to facilitate proper administration of the winter road maintenance contract. In no case shall the total expenditures for the year exceed the annual budget as approved by vote of the municipal legislative body (town meeting vote).

Selectmen recommend (3-0)

Budget Committee recommends (3-0)

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 40. To see if the Town will vote to take from surplus up to \$10,000.00 for the purpose of funding capital projects on State Aid roads and allow the balance to carry over each year.

Selectmen recommend take from surplus (3-0)

\$10,000.00

Budget Committee recommends take from surplus (3-0)

\$10,000.00

***Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands***

Article 41. To see if the Town will vote to take from surplus up to \$5,000.00 for the Contingency Fund.

Selectmen recommend take from surplus (3-0)	\$5,000.00
Budget Committee recommends take from surplus (3-0)	\$5,000.00

Motion to accept as written by Chris Chadbourne
Second by Bruce Taylor
Article approved by show of hands

Article 42. To see if the Town will authorize or direct the Board of Selectmen to use **LRAP funds** if needed.
Selectmen recommend (3-0) up to \$ 70,000.00
Budget Committee recommends (3-0) up to \$70,000.00

Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands

Article 43. To see if the Town will authorize the Board of Selectmen to use **FEMA** money where appropriate.
Selectmen recommend (3-0)
Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne
Second by Patrick Carty
Article approved by show of hands

Article 44. To see if the Town will vote to take from surplus up to \$32,000 for the purpose of developing the design and bid documents for a **proposed Fire Station**. Phase I cost Approximately \$8,000 for a conceptual design and a draft budget. Phase II Approximately \$26,000 for final design and bid document. (Some funding is available from a carry forward account).

Selectmen recommend (3-0)
Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne
Second by Kay Lyman
Article approved by show of hands

Article 45. To see if the Town will send the balance of the capital account for a Dry Hydrant to surplus.
Account balance \$1,757.94
Selectmen recommend (3-0)
Budget Committee recommends (3-0)

Motion to accept as written by Chris Chadbourne
Second by Jane Gibbons
Article approved by show of hands

Given under our hands this _____ 2021

Selectmen _____

of _____

Sweden _____

ATTEST: _____

Constables Return for Town of Sweden Oxford, SS.

Pursuant to the within warrant to me directed, I have notified and warned the inhabitants of said Town, qualified as therein expressed, to meet at said time and place and for the purposes therein named, by posting a copy of said warrant, attested by me, at the Town Hall a public and conspicuous place on _____ 2021 being at least seven days before the meeting.

Constable

Subject **FSBC, Plymouth, and Path Forward**
From <chrischadbourne@swedenmaine.org>
To Frankhowell <frankhowell@swedenmaine.org>, Julia McQueen <juliamcqueen@swedenmaine.org>
Cc Rose Mckenzie <adminassistbos@swedenmaine.org>, Dan McQueen <danmcqueen@swedenmaine.org>, Kerry Welton <kerrywelton@swedenmaine.org>
Date 2023-09-24 04:53 PM



-
- Plymouth Engineering letter 2023-08-03 from SB.pdf(~37 KB)
 - Plymouth Engineering letter 2023-09-20.pdf(~98 KB)
-

FSBC,

At our last SB meeting, on 9/13, we decided to reach out to the FSBC with updates, requests, and a suggestion for future engagement.

To bring everyone up to date on firehouse related activities over the last few weeks:

- At the request of the FSBC, the SB sent a letter, attached, to Plymouth Engineering to reestablish communications and, hopefully, restart the bidding process for the firehouse. After sending the letter, Selectman McQueen followed up with a telephone call in a further effort to reestablish communication and show interest on the part of the SB.
- Since, our last SB meeting, we have received a response from Plymouth - also attached.

Requests:

- At this point, we request that the FSBC take back responsibility for communicating with Plymouth and again take the lead on getting the bid process going.
- We also ask that the FSBC begin work with the Planning Board on the conditional use permit that will be required to build the firehouse.
- Lastly, we feel that it would be a good idea to establish some channel of regular communications between the FSBC and the SB. A monthly update - either verbal at SB meetings or written - was suggested, but we are open to other ideas.

We ask that the FSBC meet as soon as is possible and convenient to address our requests and respond.

Thank you for all the work you've done to date on the project, and we look forward to getting it back on track in the near future.

Regards

Select Board of the Town of Sweden

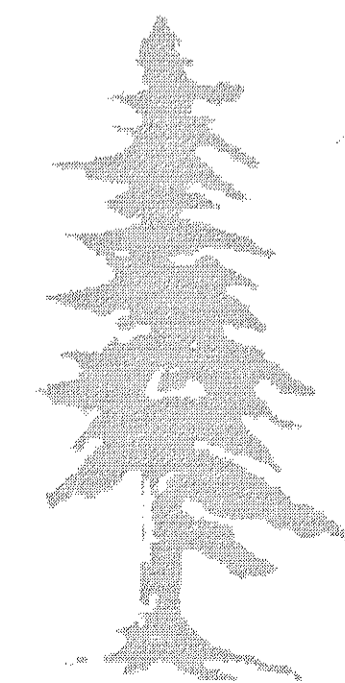
Chris Chadbourne

Dan McQueen

Kerry Welton

PROPOSED FIRE STATION TOWN OF SWEDEN

NOVEMBER, 2023
SWEDEN, MAINE

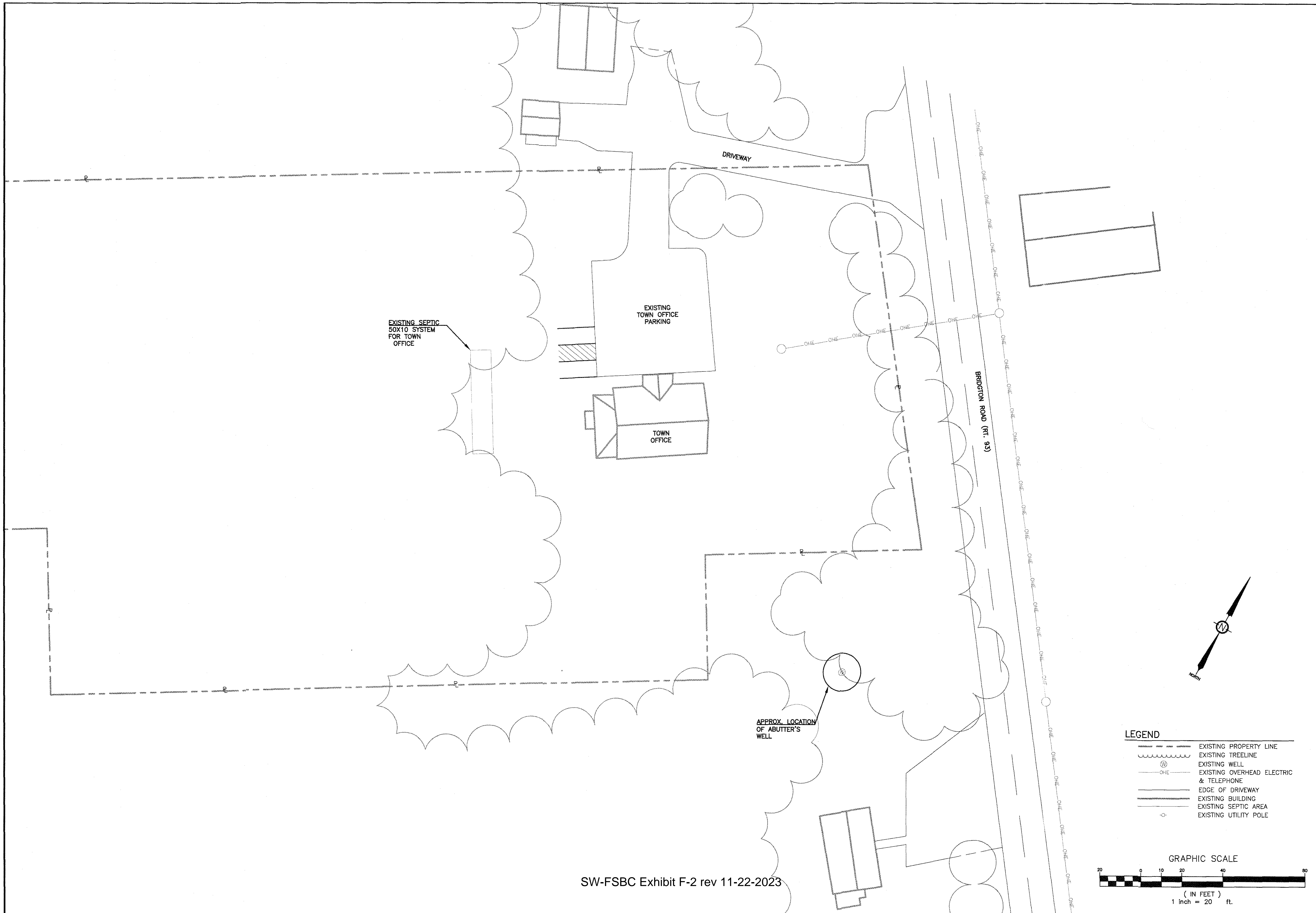


Plymouth Engineering, Inc.

8 Main Street, Unit C
Newport, Maine 04953
Tel: (207) 257-2071 Fax: (207) 257-2130
info@plymouthengineering.com
www.plymouthengineering.com

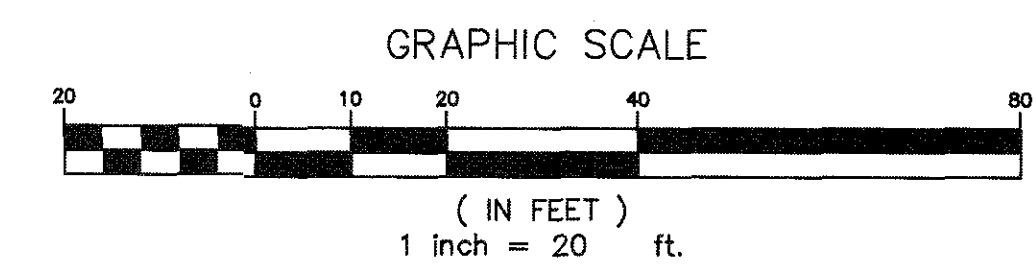
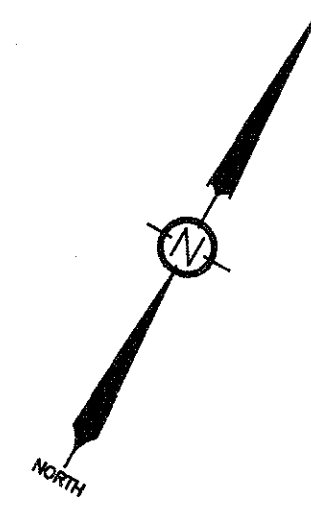
SW-FSBC Exhibit F-1 rev 11-22-2023

ISSUED FOR:		<input checked="" type="checkbox"/> PERMITTING	<input checked="" type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION	
SET NO.:					
CIVIL	EXISTING CONDITIONS EX FOUNDATION PLAN GRADING PLAN EROSION AND SEDIMENT CONTROL DETAILS CONSTRUCTION DETAILS C2 C3 C4	ARCHITECTURAL	COVER AND SHEET INDEX A101 ELEVATIONS A102 FIRST FLOOR LAYOUT PLAN A103 REFLECTIVE CEILING PLAN A104 INTERIOR ELEVATIONS A105 INTERIOR ELEVATIONS, SCHEDULES A106 SECTIONS A107	STRUCTURAL	NOTES S101 FOUNDATION PLAN S102 DETAILS S103 FRAMING PLAN S104 FRAMING PLANS DETAILS S105
		MECHANICAL	M-		



SW-FSBC Exhibit F-2 rev 11-22-2023

- LEGEND**
- EXISTING PROPERTY LINE
 - EXISTING TREELINE
 - ⊕ EXISTING WELL
 - OHE --- EXISTING OVERHEAD ELECTRIC & TELEPHONE
 - EDGE OF DRIVEWAY
 - EXISTING BUILDING
 - EXISTING SEPTIC AREA
 - EXISTING UTILITY POLE



<p>DESIGNED: Client DRAWN: TWG CHECKED: KGE APPROVED: KGE PLAN DATE: 11/21/2023 CLIENT & OWNER: TOWN OF SWEDEN 147 BRIDGTON ROAD SWEDEN, ME 04040</p>	<p>PROJECT NO. 19080 DRAWING NO. 19080C FIELDBOOK: SCALE: DATE ISSUED:</p>	<p>PROJECT NAME: NEW FIRE STATION</p>	<p>147 BRIDGTON ROAD, SWEDEN, OXFORD CITY MAINE, 04040 SHEET NAME: EXISTING CONDITIONS</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;">NO.</th> <th style="width: 5%;">DATE</th> <th style="width: 5%;">BY</th> <th style="width: 5%;">APPD.</th> <th style="width: 20%;">REVISIONS</th> <th style="width: 40%;">DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	DATE	BY	APPD.	REVISIONS	DESCRIPTION						
NO.	DATE	BY	APPD.	REVISIONS	DESCRIPTION											

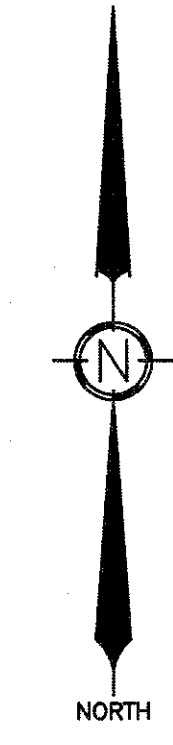
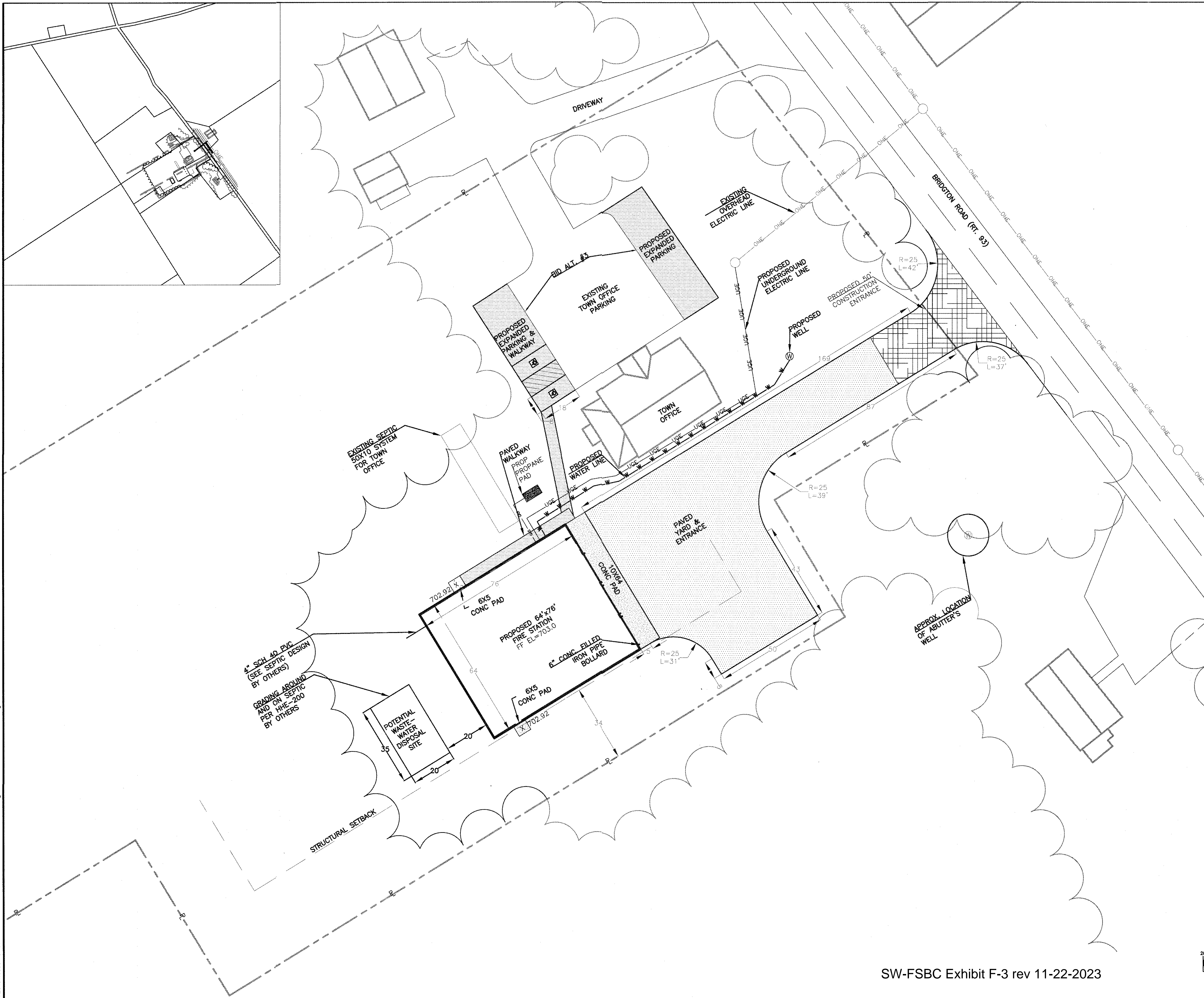
Plymouth Engineering, Inc.
 8 Main Street, 4th c
 Newport, Maine 04853
 Tel: (207) 267-2071 Fax: (207) 267-2130
 info@plymouthengineering.com
 www.plymouthengineering.com

DRAWINGS NOT SEALED ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

SHEET 1 OF 16

EX

Nov 21, 2023 - 2:28pm
 J:\2019\18080 - Sweden - Town of Sweden\Drawings\Civil\112123 19080 BASE.dwg

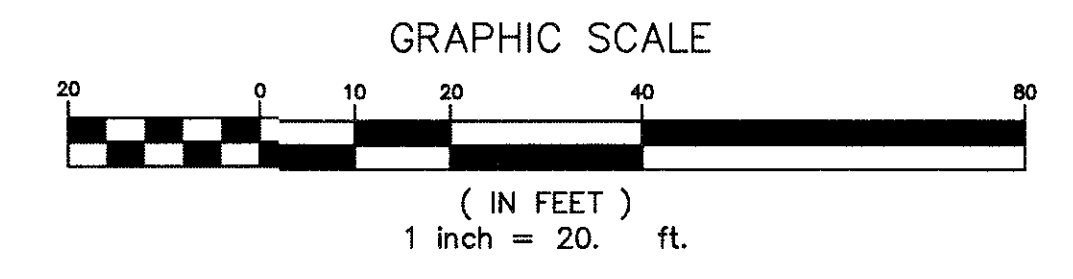


GENERAL NOTES:

- BOUNDARY AND SURVEY INFORMATION PROVIDED BY TOWN OF SWEDEN, ME.
- TOPOGRAPHY FROM LIDAR, PROVIDED BY TOWN OF SWEDEN ME.
- EXISTING PROPERTY FEATURES PROVIDED BY TOWN OF SWEDEN.
- SEPTIC AND WELL LOCATION PROVIDED BY OTHERS, SHOWN HEREIN IN CONCEPT ONLY. NO PUBLIC DRINKING WATER SUPPLY PERMITTING HAS BEEN PERFORMED.
- OVERHEAD UTILITY POLE LOCATIONS TO BE ESTABLISHED BY VERSANT POWER ALONG WITH TRANSFORMER PADS IF NEEDED FOR POWER TO UNITS.
- FEDERAL, STATE, AND MUNICIPAL LAWS AND REGULATIONS REQUIRE ANY ONE PERFORMING ANY SORT OF EXCAVATION, INCLUDING DIGGING, BORING, BACKFILLING, OR ROUGH GRADING TO NOTIFY "DIG-SAFE" (1-800-344-7233) AND ANY APPLICABLE UTILITY THAT IS NOT A PARTICIPANT IN "DIG-SAFE", AT LEAST 72 HOURS PRIOR TO COMMENCING WORK.
- FINAL LOCATION OF UTILITIES TO MEET APPLICABLE UTILITY STANDARDS.

LEGEND

- EXISTING PROPERTY LINE
- PROPOSED SETBACK LINE
- PROPOSED SEPTIC SETBACK
- PROPOSED CONTOUR
- PROPOSED EDGE OF PAVEMENT
- EXISTING TREELINE
- PROPOSED TREELINE
- EXISTING OVERHEAD ELECTRIC & TELEPHONE
- PROPOSED OVERHEAD ELECTRIC & TELEPHONE
- PROPOSED UNDERGROUND ELE. LINE
- PROPOSED SETBACK LINE
- PROPOSED SETBACK LINE
- PROPOSED CONCRETE PAD
- PROPOSED CONSTRUCTION ENTRANCE
- PROPOSED PAVEMENT AREA
- PROPOSED BUILDING
- EXISTING WELL
- PROPOSED WELL
- EXISTING UTILITY POLE



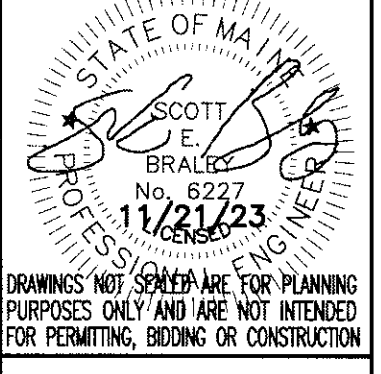
SW-FSBC Exhibit F-3 rev 11-22-2023

NO.	DATE	DESCRIPTION

PROJECT NAME: NEW FIRE STATION
 147 BRIDGTON ROAD, SWEDEN, OXFORD CITY MAINE, 04040
 SHEET NAME: SITE PLAN

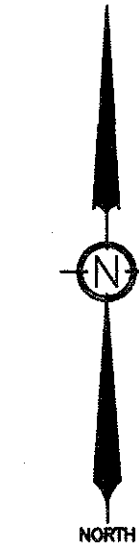
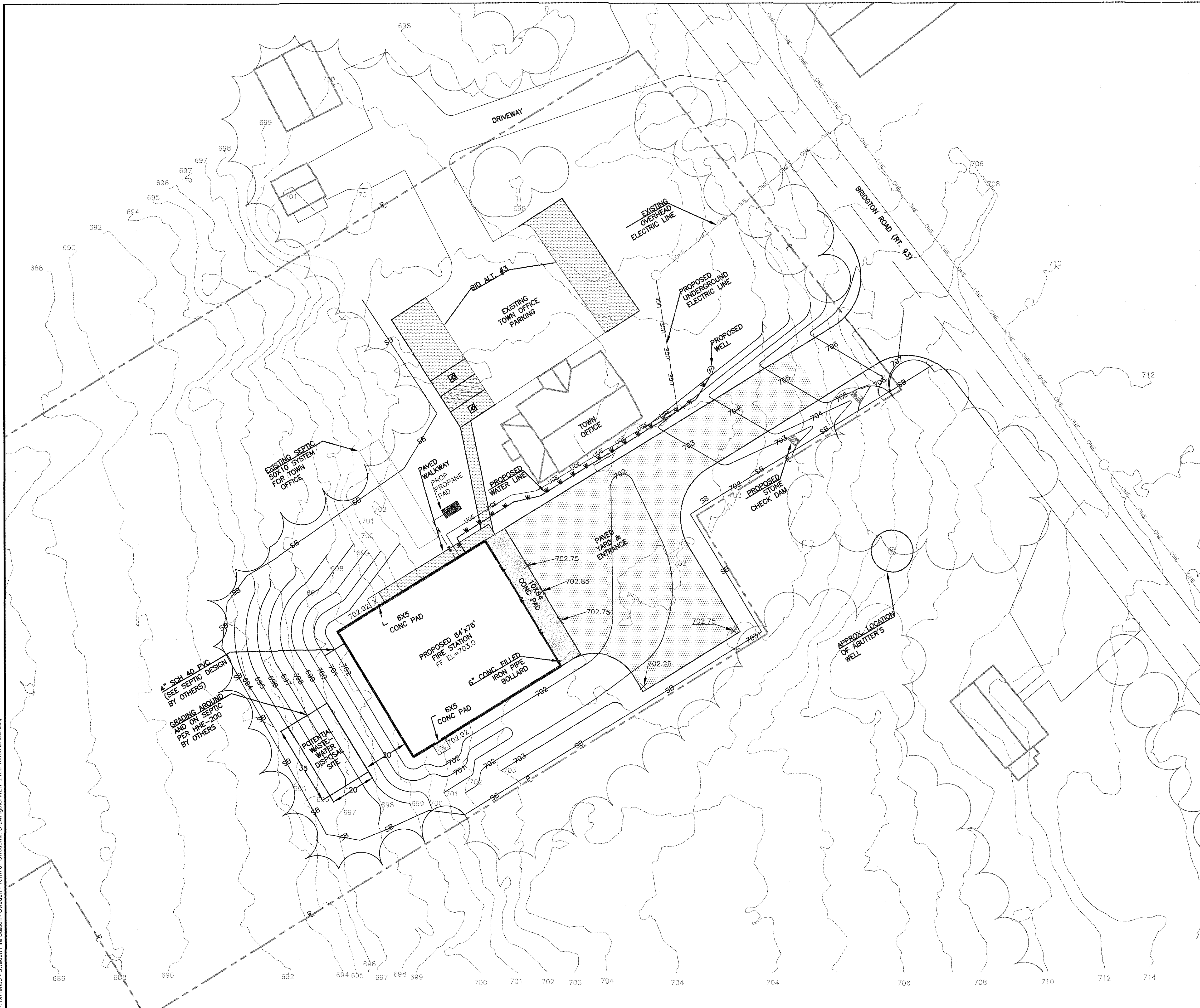
PROJECT NO. 19080
 DRAWING NO. FEEDBOOK
 SCALE: DATE ISSUED:
 DESIGNED: JLH
 CHECKED: SEB
 APPROVED: SEB
 PLAN DATE: 11/21/2023
 CLIENT & OWNER: TOWN OF SWEDEN
 147 BRIDGTON ROAD
 SWEDEN, ME 04040

Plymouth Engineering, Inc.
 8 Main Street Ste C
 Newport, Maine 04963
 Tel: (207) 257-8071 Fax: (207) 257-2130
 info@plymouthengineering.com
 www.plymouthengineering.com



DRWINGS NOT SCALE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION
 SHEET 2 OF 16
C1

Nov 21, 2023 - 2:28pm
 J:\2019\19080 - Sweden Fire Station - Sweden - Town of Sweden\J. Drawings\CIVIL\112123 19080 BASE.dwg

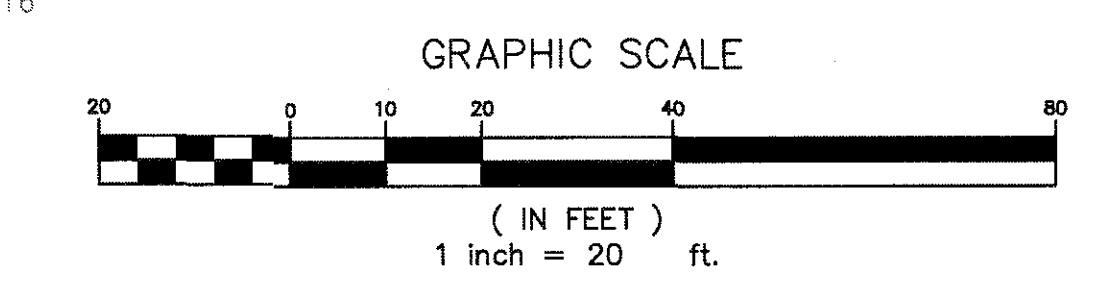


GENERAL NOTES:

1. BOUNDARY AND SURVEY INFORMATION PROVIDED BY TOWN OF SWEDEN, ME.
2. TOPOGRAPHY FROM LIDAR, PROVIDED BY TOWN OF SWEDEN, ME.
3. EXISTING PROPERTY FEATURES PROVIDED BY TOWN OF SWEDEN.
4. SEPTIC AND WELL LOCATION PROVIDED BY OTHERS, SHOWN HEREON IN CONCEPT ONLY. NO PUBLIC DRINKING WATER SUPPLY PERMITTING HAS BEEN PERFORMED.
5. OVERHEAD UTILITY POLE LOCATIONS TO BE ESTABLISHED BY VERSANT POWER ALONG WITH TRANSFORMER PADS IF NEEDED FOR POWER TO UNITS.
6. FEDERAL, STATE, AND MUNICIPAL LAWS AND REGULATIONS REQUIRE ANY ONE PERFORMING ANY SORT OF EXCAVATION, INCLUDING DIGGING, BORING, BACKFILLING, OR ROUGH GRADING TO NOTIFY "DIG-SAFE" (1-800-344-7233) AND ANY APPLICABLE UTILITY THAT IS NOT A PARTICIPANT IN "DIG-SAFE", AT LEAST 72 HOURS PRIOR TO COMMENCING WORK.
7. FINAL LOCATION OF UTILITIES TO MEET APPLICABLE UTILITY STANDARDS.

LEGEND

- EXISTING PROPERTY LINE
- PROPOSED SETBACK LINE
- PROPOSED SEPTIC SETBACK
- PROPOSED CONTOUR
- PROPOSED EDGE OF PAVEMENT
- EXISTING TREELINE
- PROPOSED TREELINE
- EXISTING OVERHEAD ELECTRIC & TELEPHONE
- PROPOSED OVERHEAD ELECTRIC & TELEPHONE
- PROPOSED UNDERGROUND ELE. LINE
- PROPOSED SETBACK LINE
- PROPOSED SETBACK LINE
- EROSION & SEDIMENT CONTROL
- PROPOSED CONCRETE PAD
- PROPOSED STONE CHECK DAM
- PROPOSED PAVEMENT AREA
- PROPOSED BUILDING
- EXISTING WELL
- PROPOSED WELL
- EXISTING UTILITY POLE

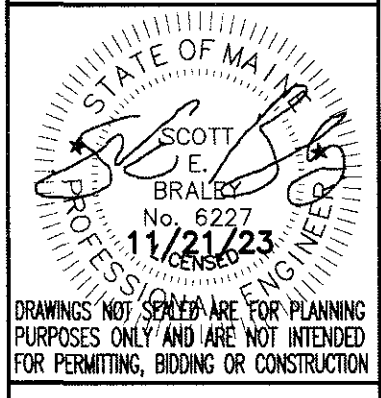


NO.	DATE	DESCRIPTION

PROJECT NAME: NEW FIRE STATION
 PROJECT NO: 19080
 DRAWING NO: 04040
 FILEBOOK: 147 BRIDGTON ROAD, SWEDEN, OXFORD CTY MAINE, 04040
 SCALE: 1/4" = 1'-0"
 DATE ISSUED: 11/21/2023
 CLIENT & OWNER: TOWN OF SWEDEN, 147 BRIDGTON ROAD, SWEDEN, ME 04040

DESIGNED: JLH
 CHECKED: SEB
 APPROVED: SEB
 PLAN DATE: 11/21/2023
 CLIENT & OWNER: TOWN OF SWEDEN, 147 BRIDGTON ROAD, SWEDEN, ME 04040

Plymouth Engineering, Jr.
 6 Main Street, etc.
 Newport, Maine 04853
 Tel: (207) 257-2071 Fax: (207) 257-2130
 info@plymouthengineering.com
 www.plymouthengineering.com



DRAWINGS NOT TO BE USED FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

SHEET 3 OF 16

C2

EROSION AND SEDIMENT CONTROL PLAN

Pre-Construction Phase
A person who conducts, or causes to be conducted, an activity that involves filling, displacing or exposing soil or other earthen materials shall take measures to prevent unreasonable erosion of soil or sediment beyond the project site or into a protected natural resource as defined in 38 M.R.S.A. § 480-B. Erosion control measures must be in place before the activity begins. Measures must remain in place and functional until the site is permanently stabilized. Adequate and timely temporary and permanent stabilization measures must be taken. The site must be maintained to prevent unreasonable erosion and sedimentation. Minimize disturbed areas and protect natural downgradient buffer areas to the extent practicable.

BMP Construction Phase
A. Sediment barriers. Prior to the beginning of any construction, properly install sediment barriers at the edge of any downgradient disturbed area or adjacent to any drainage channels within the proposed disturbed area. Maintain the sediment barriers until the disturbed area is permanently stabilized.

B. Construction entrance. Prior to any clearing or grubbing, a construction entrance shall be constructed at the intersection with the proposed access drive and the existing roadway to avoid tracking of mud, dust and debris from the site.

C. Riprap. Since riprap is used where erosion potential is high, construction must be sequenced so that the riprap is put in place with the minimum delay. Disturbance of areas where riprap is to be placed should be undertaken only when final preparation and placement of the riprap can follow immediately behind the initial disturbance. Where riprap is used for outlet protection, the riprap should be placed before or in conjunction with the construction of the pipe or channel so that it is in place when the pipe or channel begins to operate. Maintain temporary riprap, such as temporary check dams until the disturbed area is permanently stabilized.

D. Temporary stabilization. Stabilize with temporary seeding, mulch, or other non-erodible cover any exposed soils that will remain unworked for more than 14 days except, stabilize areas within 100 feet of a wetland or waterbody within 7 days or prior to a predicted storm event, whichever comes first. If hay or straw mulch is used, the application rate must be 2 bales (70-90 pounds) per 1000 sq. ft. or 1.5 to 2 tons (90-100 bales) per acre to cover 75 to 90% of the ground surface. Hay mulch must be kept moist or anchored to prevent wind blowing. An erosion control blanket or mat shall be used at the base of grassed waterways, steep slopes (15% or greater) and on any disturbed soil within 100 feet of lakes, streams and wetlands. Grading shall be planned so as to minimize the length of time between initial soil exposure and final grading. On large projects this should be accomplished by phasing the operation and completing the first phase up to final grading and seeding before starting the second phase, and so on.

E. Vegetated waterway. Upon final grading, the disturbed areas shall be immediately seeded to permanent vegetation and mulched and will not be used as outlets until a dense, vigorous vegetative cover has been obtained. Once soil is exposed for waterway construction, it should be immediately shaped, graded and stabilized. Vegetated waterways need to be stabilized early during the growing season (prior to September 15). If final seeding of waterways is delayed past September 15, emergency provisions such as sod or riprap may be required to stabilize the channel. Waterways should be fully stabilized prior to directing runoff to them.

Permanent Stabilization Defined
A. Seeded areas. For seeded areas, permanent stabilization means an 90% cover of the disturbed area with mature, healthy plants with no evidence of washing or rilling of the topsoil.

B. Sodded areas. For sodded areas, permanent stabilization means the complete binding of the sod roots into the underlying soil with no slumping of the sod or die-off.

C. Permanent mulch. For mulched areas, permanent mulching means total coverage of the exposed area with an approved mulch material. Erosion control mix may be used as mulch for permanent stabilization according to the approved application rates and limitations.

D. Riprap. For areas stabilized with riprap, permanent stabilization means that slopes stabilized with riprap have an appropriate backing of a well-graded gravel or approved geotextile to prevent soil movement from behind the riprap. Stone must be sized appropriately. It is recommended that angular stone be used.

E. Agricultural use. For construction projects on land used for agricultural purposes (E.G., pipelines across crop land), permanent stabilization may be accomplished by returning the disturbed land to agricultural use.

F. Paved areas. For paved areas, permanent stabilization means the placement of the compacted gravel subbase is completed.

G. Ditches, channels, and swales. For open channels, permanent stabilization means the channel is stabilized with mature vegetation at least three inches in height, with well-graded riprap, or with another non-erodible lining capable of withstanding the anticipated flow velocities and flow depths without reliance on check dams to slow flow. There must be no evidence of slumping of the lining, undercutting of the banks, or down-cutting of the channel.

General Construction Phase
The following erosion control measures shall be followed by the contractor throughout construction of this project:

A. All topsoil shall be collected, stockpiled, seeded with rye at 3 pounds/1,000 sq. ft. and mulched, and reused as required. Silt fencing shall be placed down gradient from the stockpiled loam. Stockpiles to be located by designation of the owner and inspecting engineer.

B. The inspecting engineer at his/her discretion, may require additional erosion control measures and/or supplemental vegetative provisions to maintain stability of earthworks and finish graded areas. The contractor shall be responsible for providing and installing any supplemental measures as directed by the inspecting engineer. Failure to comply with the engineer's directions will result in discontinuation of construction activities.

C. Erosion control mesh shall be applied in accordance with the plans over all finish seeded areas as specified on the design plans.

D. All graded or disturbed areas including slopes shall be protected during clearing and construction in accordance with the approved erosion and sediment control plan until they are adequately stabilized.

E. All erosion, and sediment control practices and measures shall be constructed, applied and maintained in accordance with the approved erosion and sediment control plan.

F. Areas to be filled shall be cleared, grubbed and stripped of topsoil to remove trees, vegetation, roots or other objectionable materials.

G. Areas shall be scarified to a minimum depth of 3 inches prior to placement of topsoil.

H. All fills shall be compacted as required to reduce erosion, slippage, settlement, subsidence or other related problems. Fill intended to support buildings, structures and conduits, etc., shall be compacted in accordance with local requirements or codes.

I. All fills shall be placed and compacted in layers not to exceed 8 inches in thickness.

J. Except for approved landfills or non-structural fills, fill material shall be free of brush, rubbish, rocks, logs, stumps, building debris and other objectionable materials that would interfere with or prevent construction of satisfactory lifts.

K. Frozen material or soft, mucky or highly compressible materials shall not be incorporated into fill slopes or structural fills.

L. Fill shall not be placed on a frozen foundation.

M. Seeps or springs encountered during construction shall be handled appropriately.

N. All graded areas shall be permanently stabilized immediately following finished grading.

O. Remove any temporary control measures, such as silt fence, within 30 days after permanent stabilization is obtained. Remove any accumulated sediments and stabilize.

Permanent Vegetation
Permanent vegetative cover should be established on disturbed areas where permanent, long lived vegetative cover is needed to stabilize the soil, to reduce damages from sediment and runoff, and to enhance the environment.

Seedbed Preparation
A. Grade as feasible to permit the use of conventional equipment for seedbed preparation, seeding, mulch application and anchoring, and maintenance.

B. Apply limestone and fertilizer according to soil tests such as those offered by the University of Maine soil testing laboratory. Soil sample matters are available from the local cooperative extension service office. If soil testing is not feasible on small or variable sites, or where timing is critical, fertilizer may be applied at the rate of 800 pounds per acre or 18.4 pounds per 1,000 square feet using 10-20-20 (n-p2o5-k2o) or equivalent. Apply ground limestone (equivalent to 50% calcium plus magnesium oxide) at a rate of 3 tons per acre (138 lb. Per 1,000 sq. Ft.).

C. Work lime and fertilizer into the soil as nearly as practical to a depth of 4 inches with a disc, spring tooth harrow or other suitable equipment. The final harrowing operation should be on the general contour. Continue tillage until a reasonably uniform, fine seedbed is prepared. All but clay or silty soils and coarse sands should be rolled to firm the seedbed where feasible. D. Seeding cannot be done within the seeding dates, mulch application. Remove all other debris, such as wire, cable, tree roots, concrete, clods, lumps or other unsuitable material.

E. Inspect seedbed just before seeding. If traffic has left the soil compacted, the area must be tilled and firmed as above.

F. Permanent seeding should be made 45 days prior to the first killing frost or as a dormant seeding with mulch after the first killing frost and before snowfall. When crown vetch is seeded in late summer, at least 35% of the seed should be hard seed (unscarified). If seeding cannot be done within the seeding dates, mulch according to the temporary mulching BMP and overwinter stabilization and construction to protect the site and delay seeding until the next recommended seeding period.

G. Following seed bed preparation, swale areas, fill areas and back slopes shall be seeded at a rate of 3 lbs./1,000 s.F. With a mixture of 35% creeping red h. Fescue, 6% red top, 24% Kentucky bluegrass, 10% perennial ryegrass, 20% annual ryegrass and 5% white Dutch clover.

I. Areas which have been temporarily or permanently seeded shall be mulched immediately following seeding.

J. Areas which cannot be seeded within the growing season shall be mulched for over-winter protection and the area should be seeded at the beginning of the growing season.

Water Construction Phase
A. Permanent stabilization consists of at least 90% vegetation, pavement/gravel base or riprap.

B. Do not expose slopes or leave slopes exposed over the winter or for any other extended time of work suspension unless fully protected with mulch.

C. Apply hay mulch at twice the standard rate (150 lbs. Per 1,000 sq. ft.). The mulch must be thick enough such that the ground surface will not be visible and must be anchored.

D. Use mulch and mulch netting or an erosion control mulch blanket or all slopes greater than 8% or other areas exposed to direct wind.

E. Install an erosion control blanket in all drainageways (bottom and sides) with a slope greater than 3%.

F. See the vegetation measures for more information on seeding dates and types.

G. Winter excavation and earthwork shall be completed so that no more than 1 acre of the site is without stabilization at any one time.

H. An area within 100 feet of a protected natural resource must be protected with a double row of sediment barrier.

I. Temporary mulch must be applied within 7 days of soil exposure or prior to any storm event, but after every workday in areas within 100 feet from a protected natural resource.

J. Areas that have been brought to final grade must be permanently mulched that same day.

K. If snowfall is greater than 1 inch (fresh or cumulative), the snow shall be removed from the areas due to be seeded and mulched.

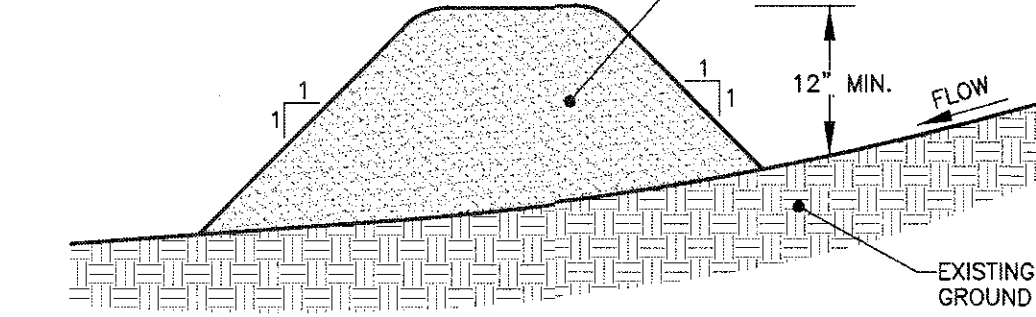
L. Loam shall be free of frozen clumps before it is applied.

M. All vegetated ditch lines that have not been stabilized by November 1, or will be worked during the winter construction period, must be stabilized with an appropriate stone lining backed by an appropriate gravel bed or geotextile unless specifically released from this standard by the department.

Maintenance and Inspection Phase
A. Contractor shall inspect disturbed and impervious areas, and erosion and stormwater control measures, areas used for storage that are exposed to precipitation, and locations where vehicles enter or exit the parcel at least once a week and before and after a storm event, prior to completion of permanent stabilization. A person with knowledge of erosion and stormwater must conduct the inspection. This person must be identified in the inspection log. If best management practices (BMPs) need to be modified or if additional BMPs are necessary, implementation must be completed within 7 calendar days and prior to any storm event (rainfall). All measures must be maintained in effective operating condition until areas are permanently stabilized.

B. A log (report) must be kept summarizing the scope of the inspection, name(s) and qualifications of the personnel making the inspection, the date(s) of the inspection, and major observations relating to operation of erosion and sedimentation controls and pollution prevention measures. Major observations must include: BMPs that need to be maintained; location(s) of BMPs that failed to operate as designed or proved inadequate for a particular location; and location(s) where additional BMPs are needed that did not exist at the time of inspection. Follow-up to correct deficiencies or enhance controls must also be indicated in the log and dated, including what action was taken and when.

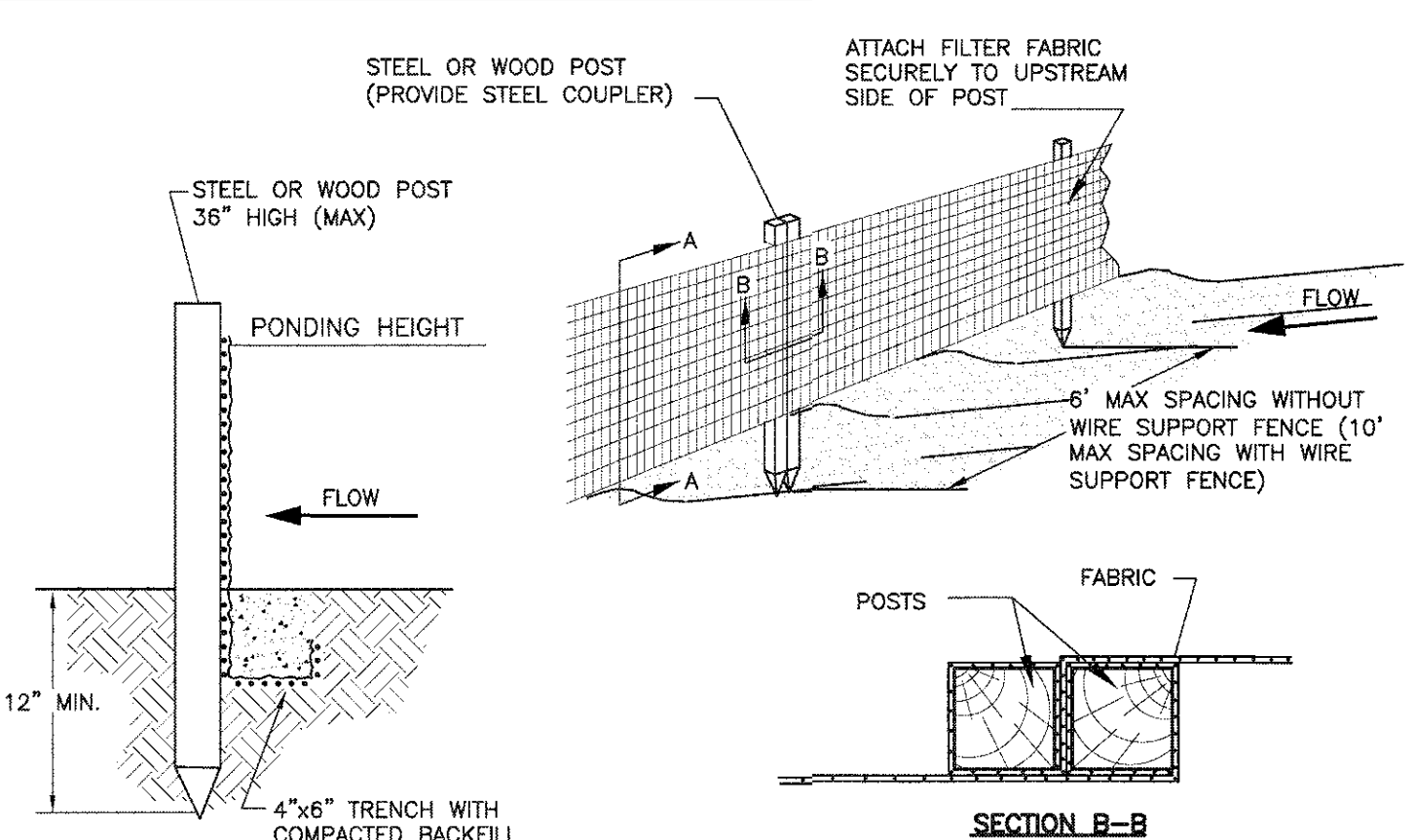
NOTE:
DURING PERIODS OF WINTER CONSTRUCTION (NOV. 15 THROUGH APRIL 15), THE CONTRACTOR SHALL INSTALL EROSION CONTROL MIX BERMS IN LIEU OF SILT FENCE.



EROSION CONTROL MIX:
EROSION CONTROL MIX SHALL CONTAIN A WELL-GRADED MIXTURE OF PARTICLE SIZES & MAY CONTAIN ROCKS LESS THAN 4" IN DIAMETER. EROSION CONTROL MIX MUST BE FREE OF REFUSE, PHYSICAL CONTAMINANTS, AND MATERIAL TOXIC TO PLANT GROWTH. THE MIX COMPOSITION SHALL MEET THE FOLLOWING STANDARDS:
- THE ORGANIC MATTER CONTENT SHALL BE BETWEEN 90% - 100% DRY WEIGHT BASIS
- PARTICLE SIZE BY WEIGHT SHALL BE 100% PASSING A 6" SCREEN AND A MINIMUM OF 70% MAXIMUM OF 85% PASSING A 0.75" SCREEN
- THE ORGANIC PORTION NEEDS TO BE FIBROUS AND ELONGATED
- LARGE PORTIONS OF SILTS, CLAYS OR FINE SANDS ARE NOT ACCEPTABLE IN THE MIX.
- SOLUBLE SALTS CONTENT SHALL BE < 4.0 mmhos/cm.
- pH SHALL FALL BETWEEN 5.0 - 8.0.

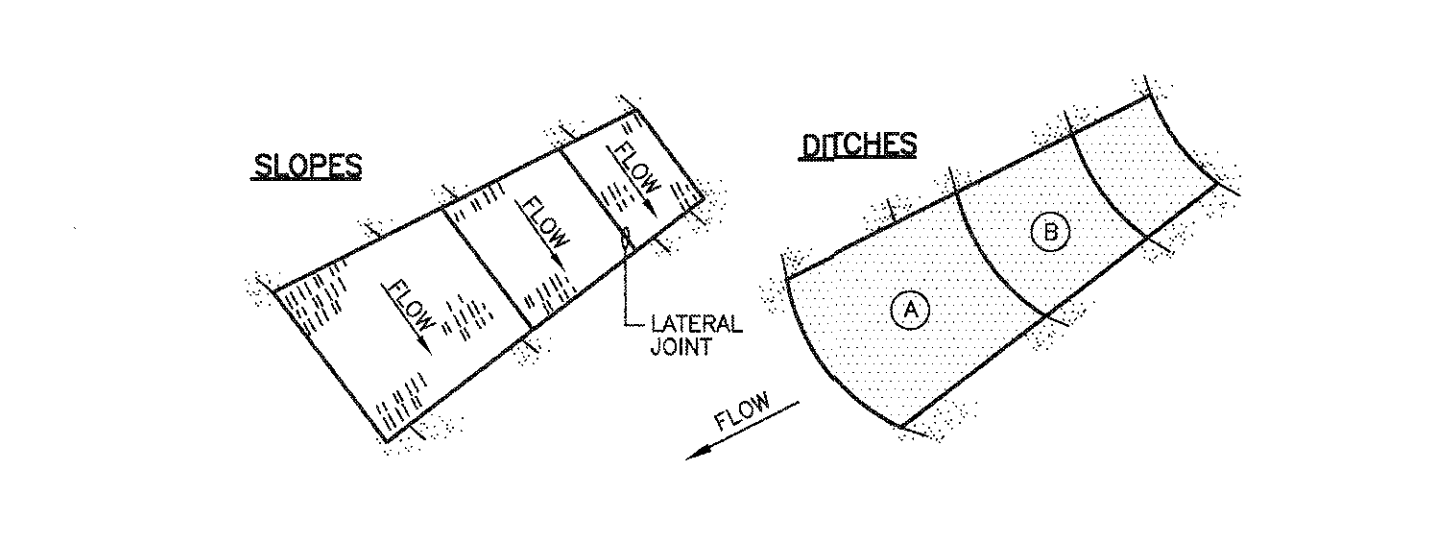
EROSION CONTROL MIX BERM

NOT TO SCALE



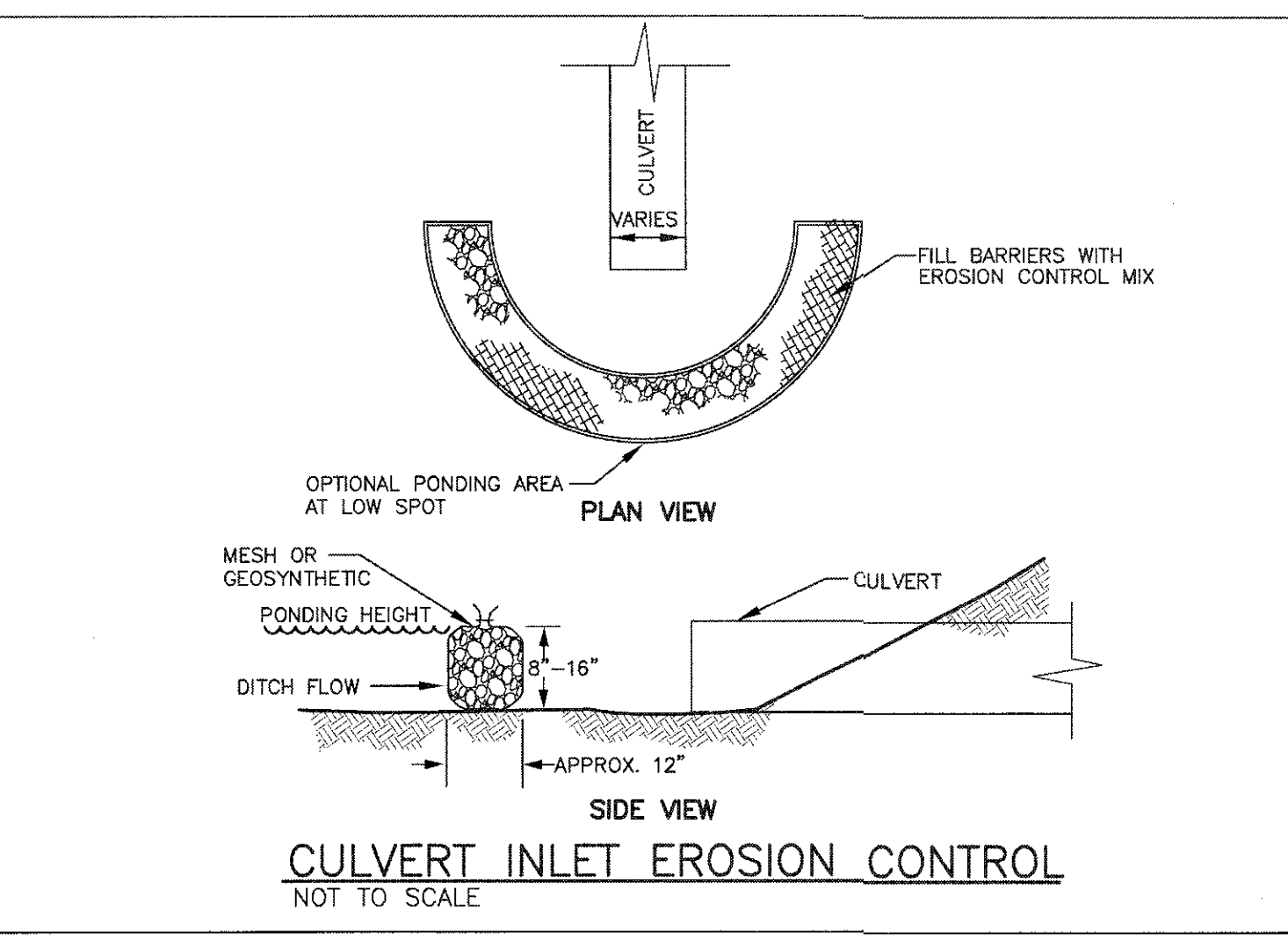
SILT FENCE

NOT TO SCALE



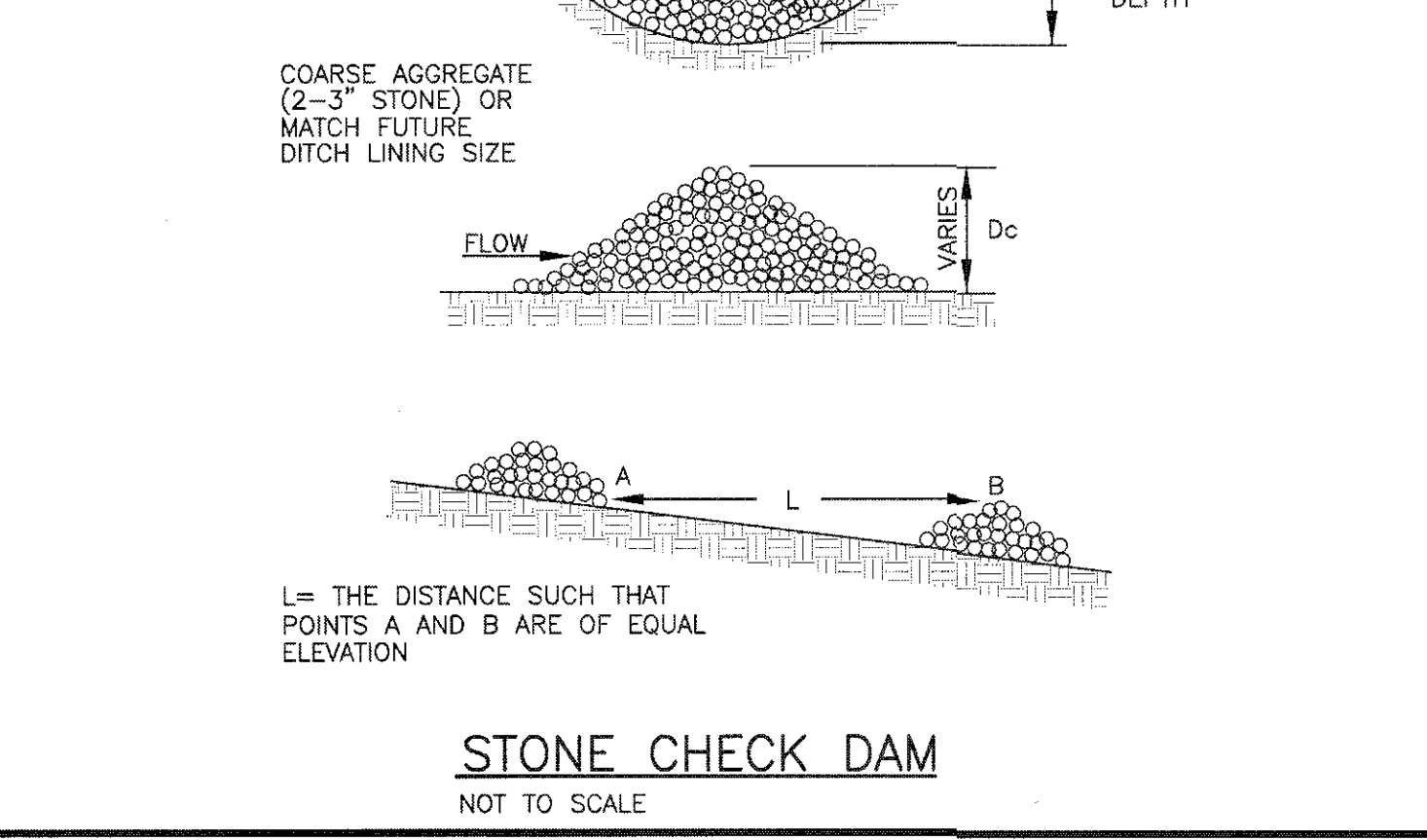
EROSION CONTROL BLANKET

NOT TO SCALE



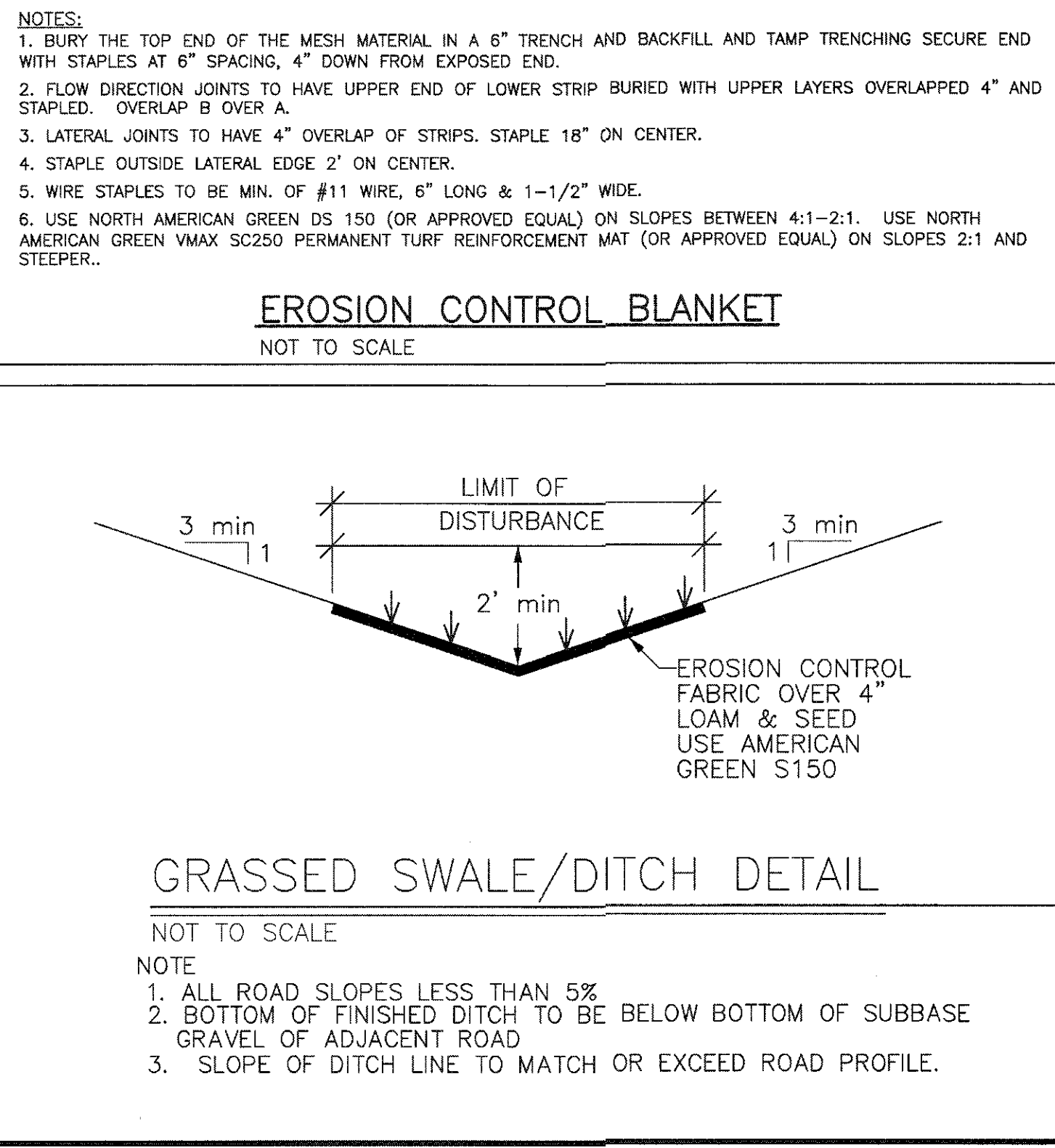
CULVERT INLET EROSION CONTROL

NOT TO SCALE



STONE CHECK DAM

NOT TO SCALE

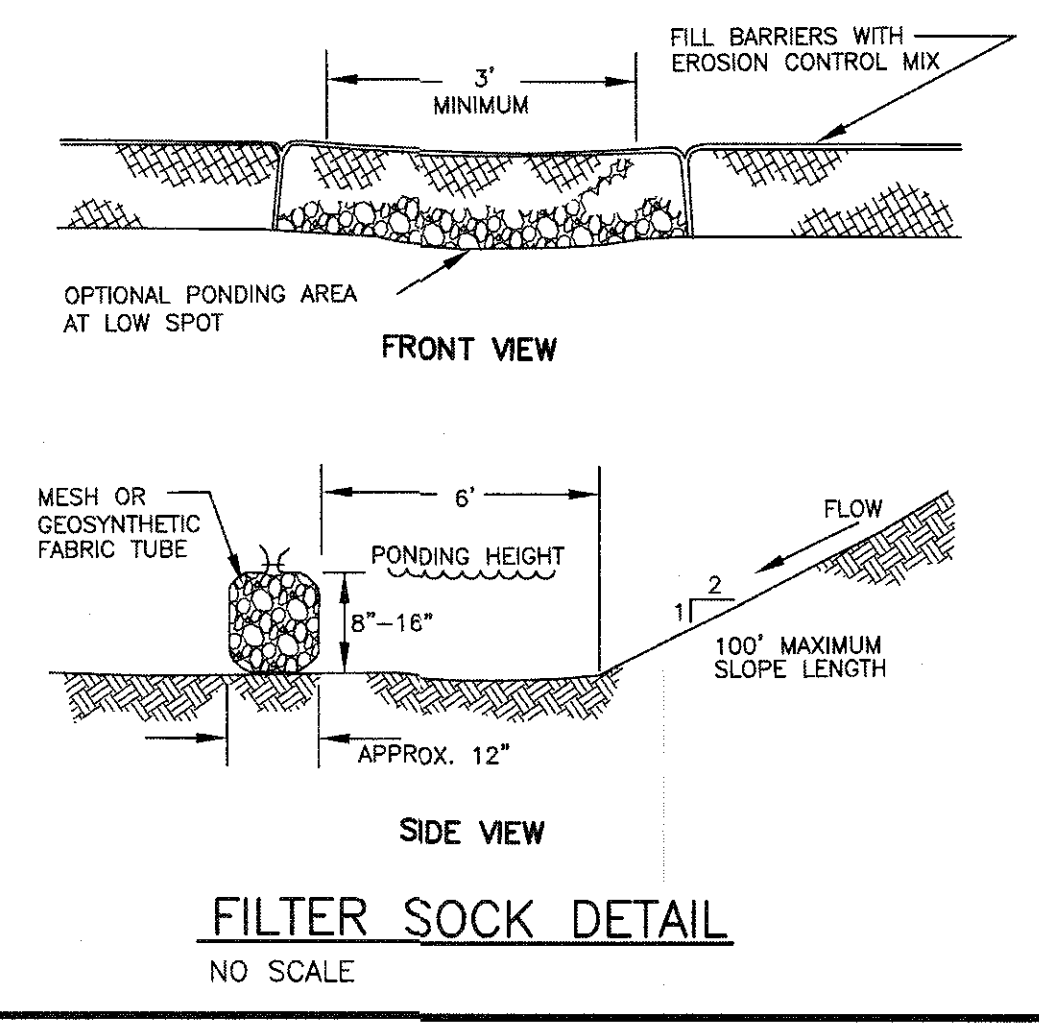


GRASSED SWALE/DITCH DETAIL

NOT TO SCALE

CONSTRUCTION NOTES:

- All work shall conform to the applicable codes and ordinances.
- Contractor shall visit the site and familiarize him or herself with all conditions affecting the proposed work and shall make provisions as to the cost thereof. Contractor shall be responsible for familiarizing him or herself with all contract documents, field conditions and dimensions and confirming that the work may be accomplished as shown prior to proceeding with construction. Any discrepancies shall be brought to the attention of the engineer prior to the commencement of work.
- Contractor shall notify engineer of all products or items noted as "existing" which are not found in the field.
- Install all equipment and materials in accordance with manufacturer's recommendations and owner's requirements unless specifically otherwise indicated or where local codes or regulations take precedence.
- Contractor shall verify all dimensions and conditions in the field prior to fabrication and erection of any material. Any unusual conditions shall be reported to the attention of the engineer.
- Contractor shall clean and remove debris and sediment deposited on public streets, sidewalks, adjacent areas, or other public ways due to construction.
- Contractor shall incorporate provisions as necessary in construction to protect existing structures, physical features, and maintain site stability during construction. Contractor shall restore all areas to original condition and as directed by design drawings.
- Site contractor shall obtain all required permits prior to construction.
- All erosion and sediment control measures shall be installed in accordance with "Maine Erosion and Sedimentation Control Handbook for Construction: Best Management Practices" published by the Cumberland County Soil and Water Conservation District and Maine Department of Environmental Protection, March 2004 or latest edition. It shall be the responsibility of the contractor to possess a copy of the erosion control plan at all times.
- The contractor is hereby cautioned that all site features shown hereon are based on field observations by the surveyor and by information provided by utility companies. The information is not to be relied on as being exact or complete. The contractor shall contact Dig Safe (1-888-digsafe) at least three (3) but not more than thirty (30) days prior to commencement of excavation or demolition to verify horizontal and vertical location of all utilities.
- Contractor shall be aware that Dig Safe only notifies its "member" utilities about the dig. When notified, Dig Safe will advise contractor of member utilities in the area. Contractor is responsible for identifying and contacting non-member utilities directly. Non-member utilities may include town or city water and sewer districts and small local utilities, as well as USG public works systems.
- Contractors shall be responsible for compliance with the requirements of 23 M.R.S.A. 3360-A. It shall be the responsibility of the contractor to coordinate with the appropriate utilities to obtain authorization prior to relocation of any existing utilities which conflict with the proposed improvements shown on these plans. If a utility conflict arises, the contractor shall immediately notify the owner, the municipality and appropriate utility company prior to proceeding with any relocation.
- All pavement markings and directional signage shown on the plan shall conform to the Manual of Uniform Traffic Control Devices (MUTCD) standards.
- All pavement joints shall be sawcut prior to paving to provide a durable and uniform joint.
- No holes, trenches or structures shall be left open overnight in any excavation accessible to the public or in public rights-of-way.
- All work within the public right-of-way shall require a M.D.O.T. Permit as well as permits from the town as applicable.
- The proposed limits of clearing shown hereon are approximate based upon the proposed limits of site grading. The applicant reserves the right to perform normal forest management activities outside of the clearing limit as shown. Tree removal outside of the limits of clearing may be necessary to remove dead or dying trees or tree limbs. This removal is due to potential safety hazards and to promote proper forest growth.
- Immediately upon completion of cuts/fills, the contractor shall stabilize disturbed areas in accordance with erosion control notes and as specified on plans.
- The contractor shall be fully and solely responsible for the removal, replacement and rectification of all damaged or defective material and workmanship in connection with the contract work. The contractor shall replace or repair as directed by the owner all such damaged or defective materials which appear within a period of one year from the date of substantial completion.
- All work performed by the general contractor and/or trade subcontractor shall conform to the requirements of local, state or federal laws, as well as any other governing requirements, whether or not specified on the drawings.
- Where the terms "approved equal", "other approved", "equal to", "acceptable" or other general qualifying terms are used in these notes, it shall be understood that reference is made to the ruling and judgment of the owner and/or owner's engineer.
- The general contractor shall provide all necessary protection for the work until turned over to the owner.
- The general contractor shall maintain a current and complete set of construction drawings on site during all phases of construction for use of all trades.
- The contractor shall take full responsibility for any changes and deviation of approved plans not authorized by the architect/engineer and/or client/owner.
- Details are intended to show end result of design. Any modification to suit field dimension and condition shall be submitted to the engineer for review and approval prior to any work.
- Before the final acceptance of the project, the contractor shall remove all equipment and materials, repair or replace private or public property which may have been damaged or destroyed during construction, clean the areas within and adjacent to the project which may have been obstructed by his/her operations, and leave the project area neat and presentable.



FILTER SOCK DETAIL

NO SCALE

REVISIONS

NO.	DATE	DESCRIPTION

PROJECT NAME: NEW FIRE STATION
 PROJECT NO.: 19080
 DRAWING NO.: 04040
 FIELDBOOK: 147 BRIDGTON ROAD, SWEDEN, OXFORD CITY, MAINE, 04040
 SCALE: 1"=40'
 DATE ISSUED: 11/21/2023
 TOWN OF SWEDEN
 447 BRIDGTON ROAD
 SWEDEN, ME 04040

DESIGNED: ALH
 DRAWN: ALH
 CHECKED: SEB
 APPROVED: JJJ

PROJECT NO. 19080
 DRAWING NO. 04040
 FIELDBOOK: 147 BRIDGTON ROAD, SWEDEN, OXFORD CITY, MAINE, 04040
 SCALE: 1"=40'
 DATE ISSUED: 11/21/2023
 TOWN OF SWEDEN
 447 BRIDGTON ROAD
 SWEDEN, ME 04040

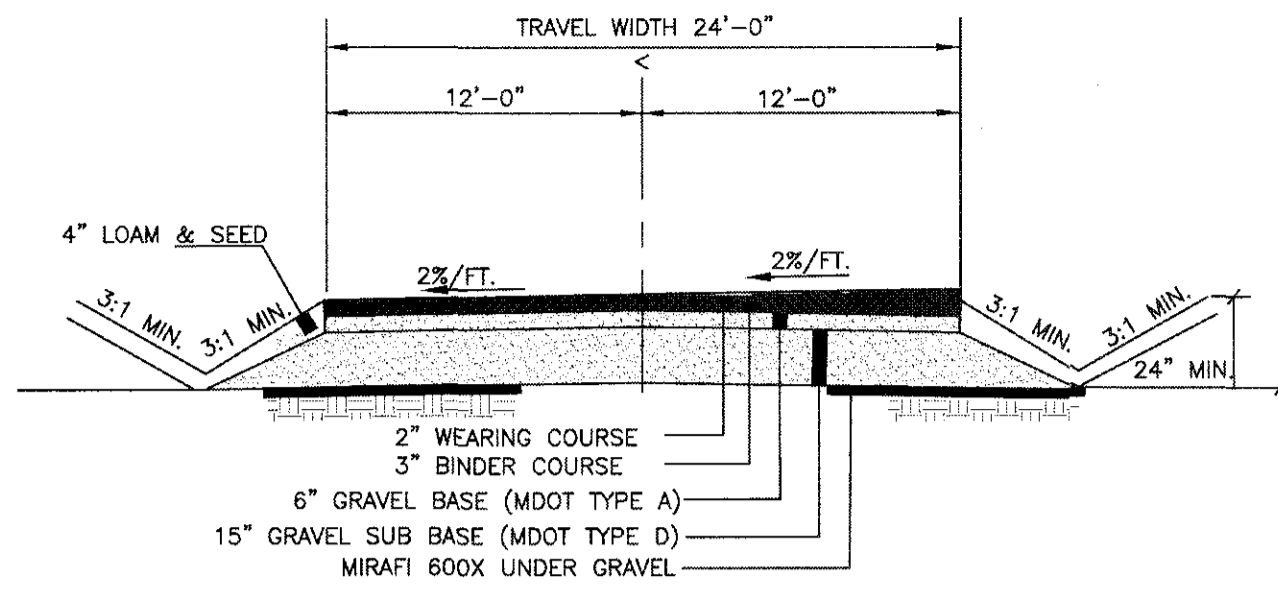
plymouth engineering, inc.
 8 Main Street, etc c
 Newport, Maine 04863
 Tel: (207) 257-2071 Fax: (207) 257-2130
 info@plymouthengineering.com
 www.plymouthengineering.com

STATE OF MAINE
 SCOTT J. BRALEY
 No. 6227
 11/21/23
 DRAWINGS NOT TO SCALE ARE FOR PLANNING PURPOSES ONLY AND ARE NOT INTENDED FOR PERMITTING, BIDDING OR CONSTRUCTION

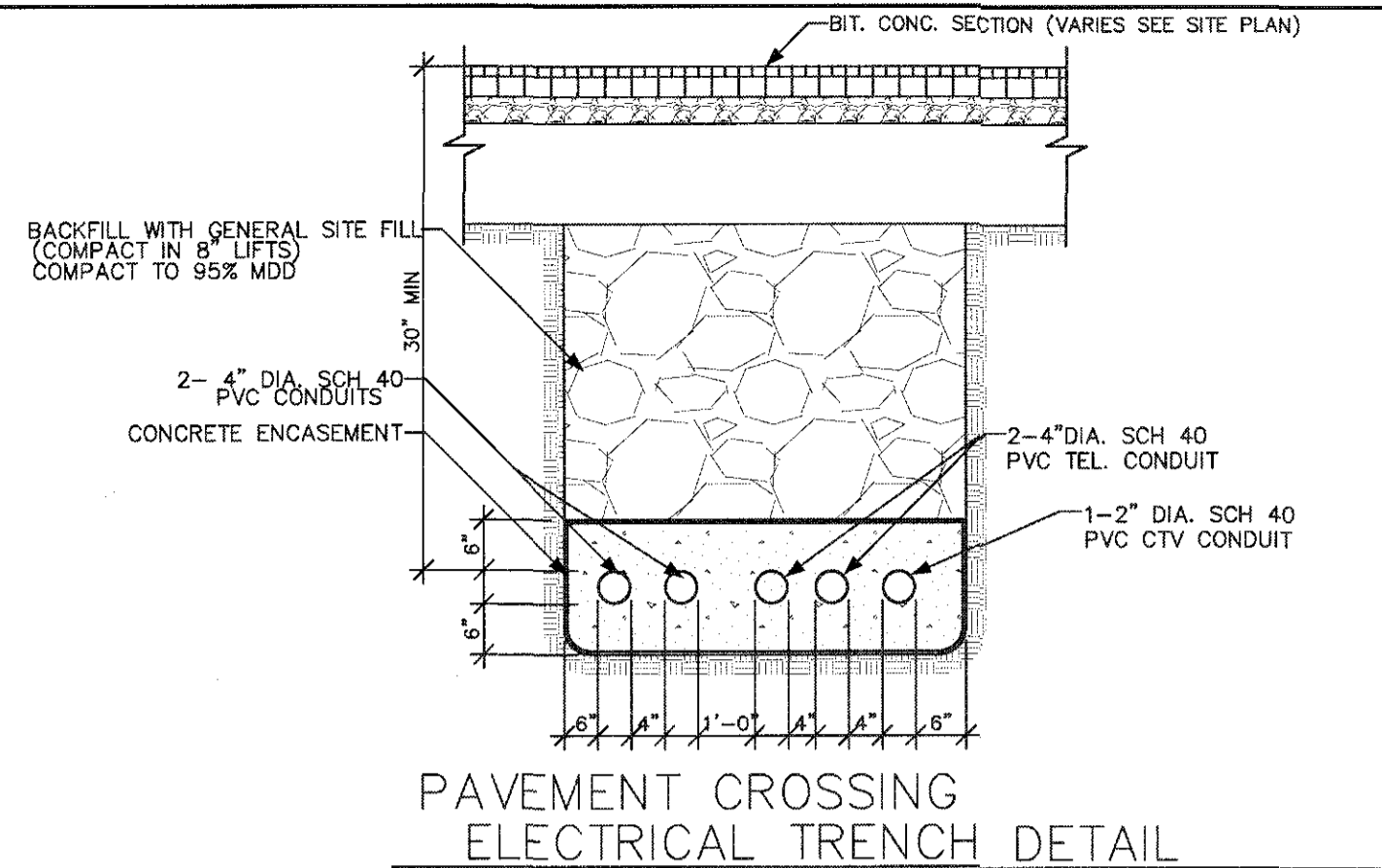
SHEET 4 OF 16

C3

Nov 21, 2023, 2:28pm
 J:\2019\19080 - Sweden Fire Station - Sweden - Town of Sweden\Drawings\CIVIL\112123_19080 BASE.dwg

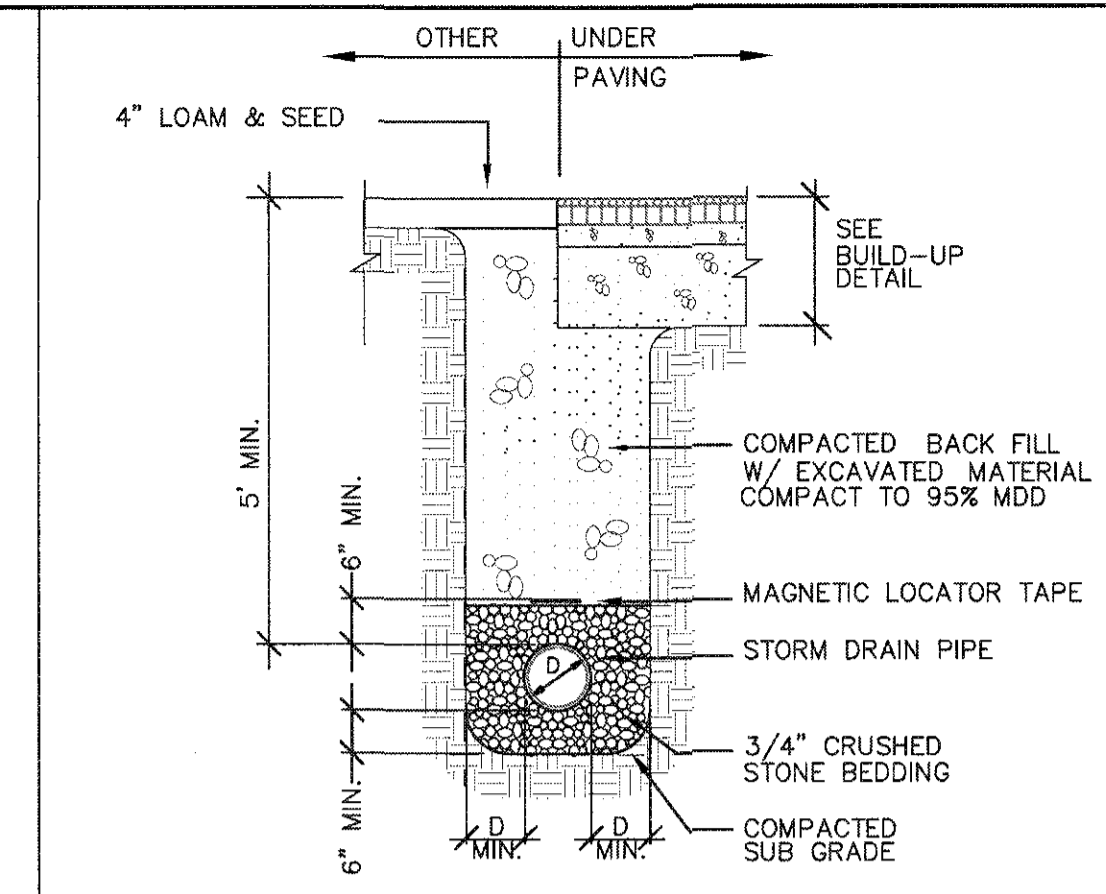


DRIVEWAY ACCESS (DRIVEWAY STANDARD)
NOT TO SCALE



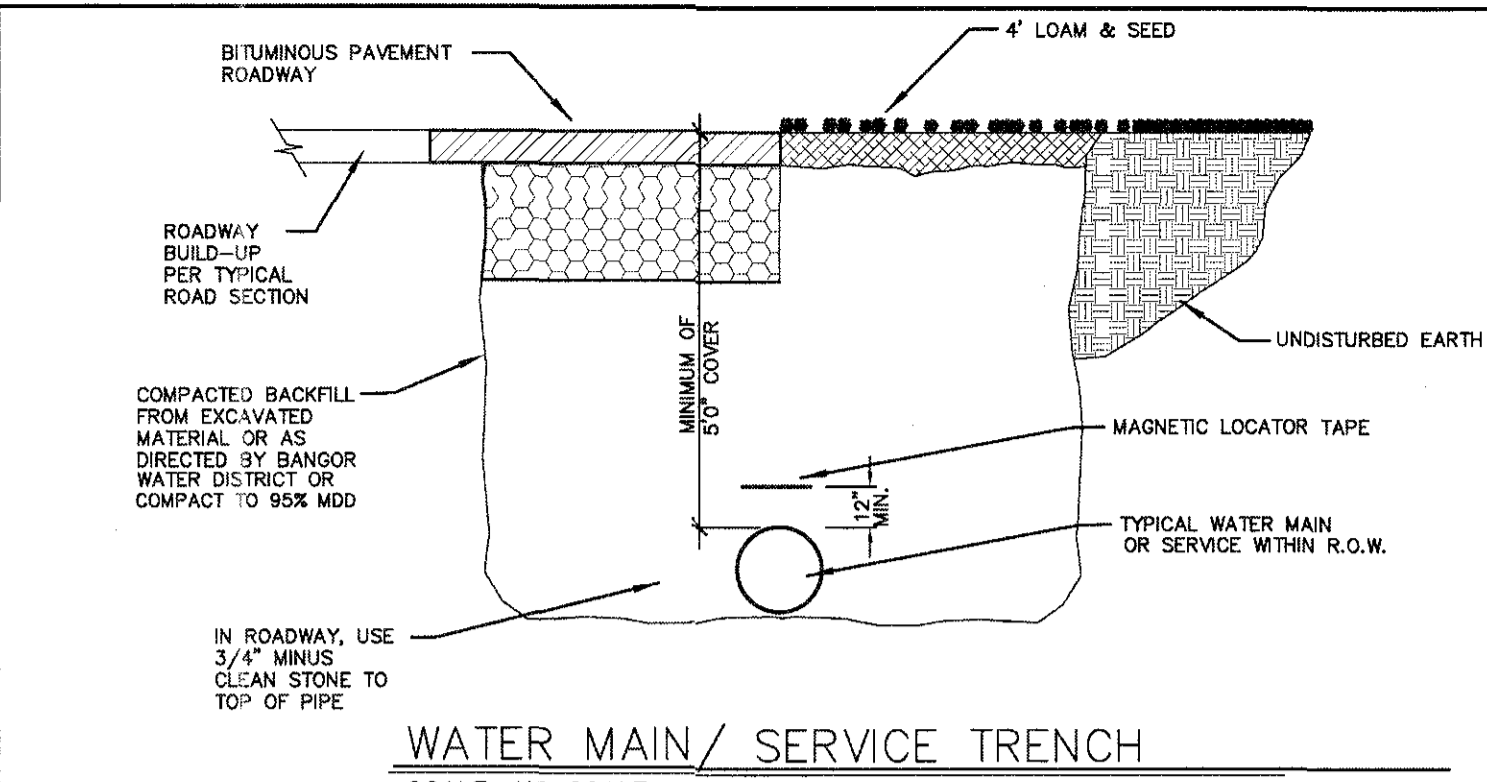
PAVEMENT CROSSING ELECTRICAL TRENCH DETAIL

NO SCALE
NOTES:
NUMBER OF CONDUIT, MATERIAL, SIZE, & SEPARATION TO BE VERIFIED AT UTILITY PRECONSTRUCTION MEETING.



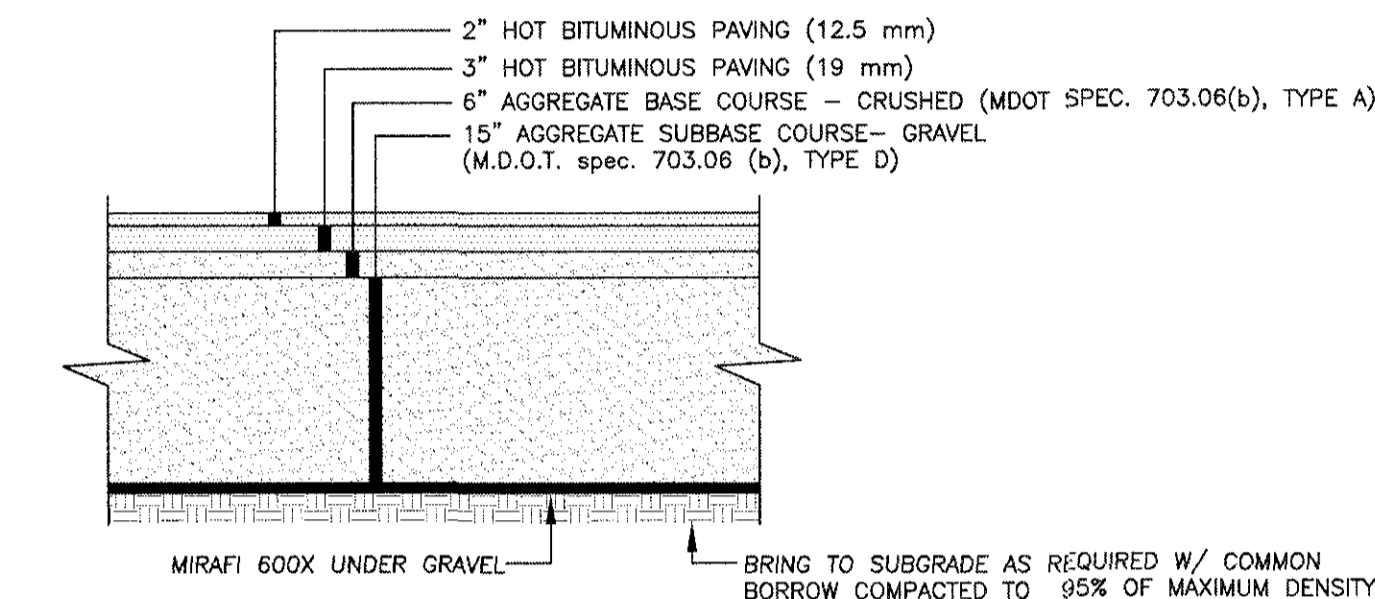
STORM DRAIN/SANITARY SEWER TRENCH DETAIL

NOT TO SCALE
NOTES:
1. ALL BACKFILL PROCEDURES MUST BE PROVIDED USING MAXIMUM 8" LIFTS, LOOSE MEASURE
2. ANY LINE TO BE PLACED WITH LESS THAN 5" OF COVER MUST BE AUTHORIZED BY THE OWNER. ANY LINE PROVIDED WITH LESS THAN 4.5" OF COVER MUST BE INSULATED WITH 2" OF RIGID INSULATION, WIDTH TO BE DETERMINED DEPENDING UPON DEPTH & LOCATION.
3. ANY LINE CROSSING OF OTHER UTILITIES WHERE THE VERTICAL SEPARATION DISTANCE IS LESS THAN 2 FEET SHALL BE INSULATED WITH 2" OF RIGID INSULATION, IN ALL DIRECTIONS FOR A DISTANCE OF 2' FROM THE CROSSING. (SEE DETAIL).



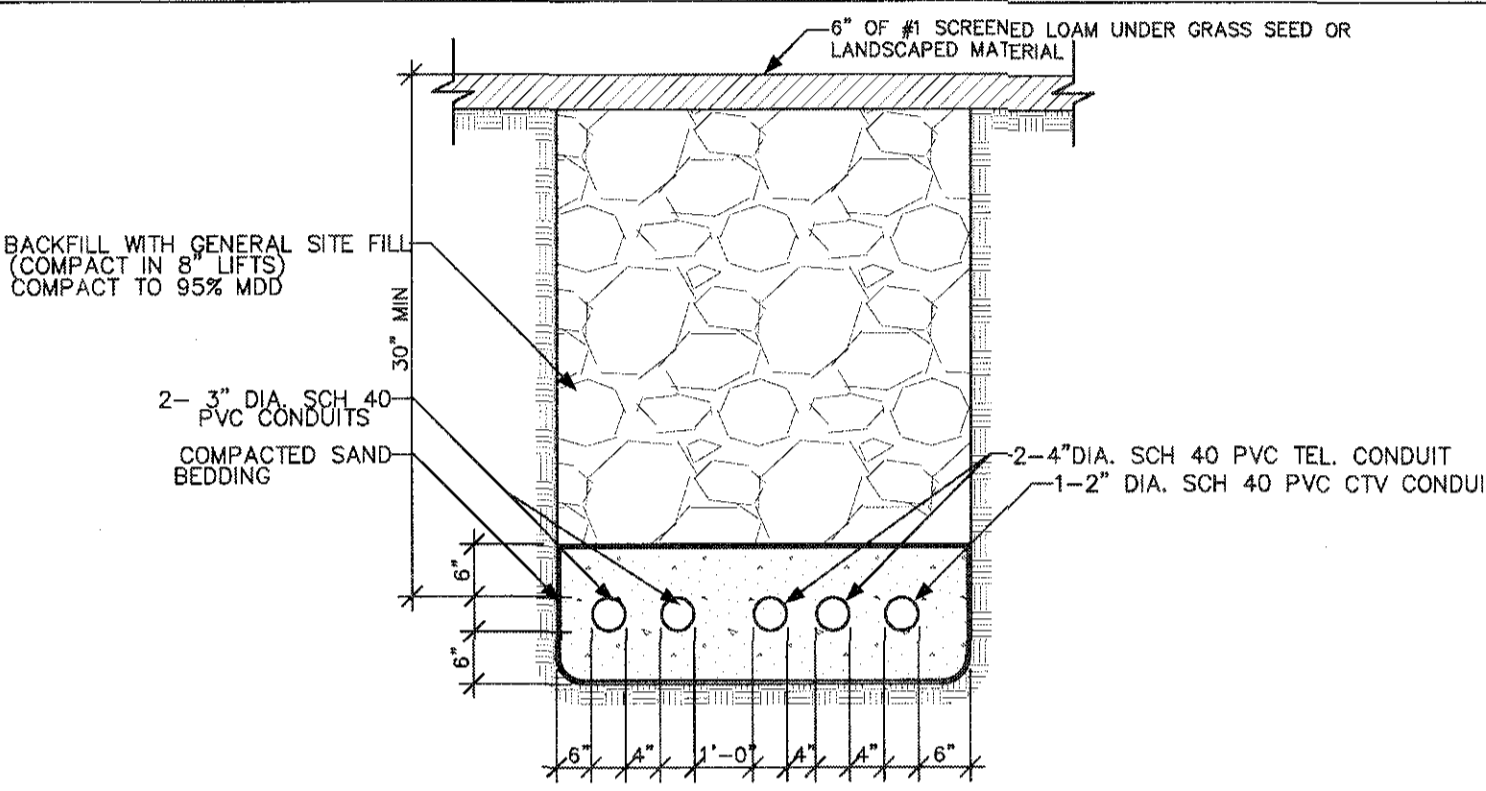
WATER MAIN/ SERVICE TRENCH

SCALE: NO SCALE
NOTES:
1. ALL BACKFILL PROCEDURES MUST BE COMPLETED USING MAXIMUM 12" COMPACTED LIFTS
2. ANY WATER LINE TO BE PLACED WITH LESS THAN 5" OF COVER MUST BE AUTHORIZED BY THE OWNER. ANY WATER LINE PROVIDED WITH LESS THAN 4" OF COVER MUST BE INSULATED WITH 2" OF RIGID INSULATION. WIDTH TO BE DETERMINED DEPENDING UPON DEPTH & LOCATION.
3. ANY WATER LINE CROSSING OF OTHER UTILITIES WHERE THE VERTICAL SEPARATION DISTANCE IS LESS THAN 2 FEET SHALL BE INSULATED WITH 2" OF RIGID INSULATION, IN ALL DIRECTIONS FOR A DISTANCE OF 2' FROM THE CROSSING.



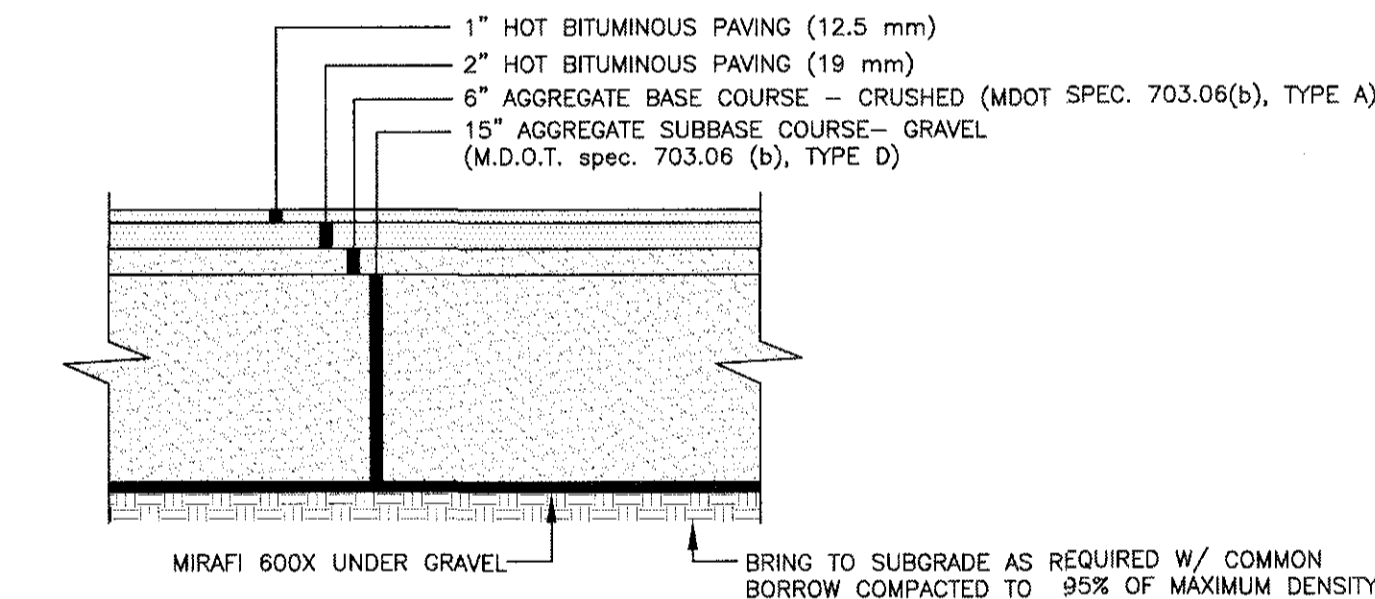
NOTES:
1. COMPACT GRAVEL SUBBASE, BASE COURSE TO 92% OF MAXIMUM DENSITY USING HEAVY ROLLER COMPACTION.
2. CONTRACTOR SHALL SET GRADE STAKES MARKING SUBBASE AND FINISH GRADE ELEVATIONS FOR CONSTRUCTION REFERENCE.

HEAVY DUTY BITUMINOUS PAVEMENT
NOT TO SCALE



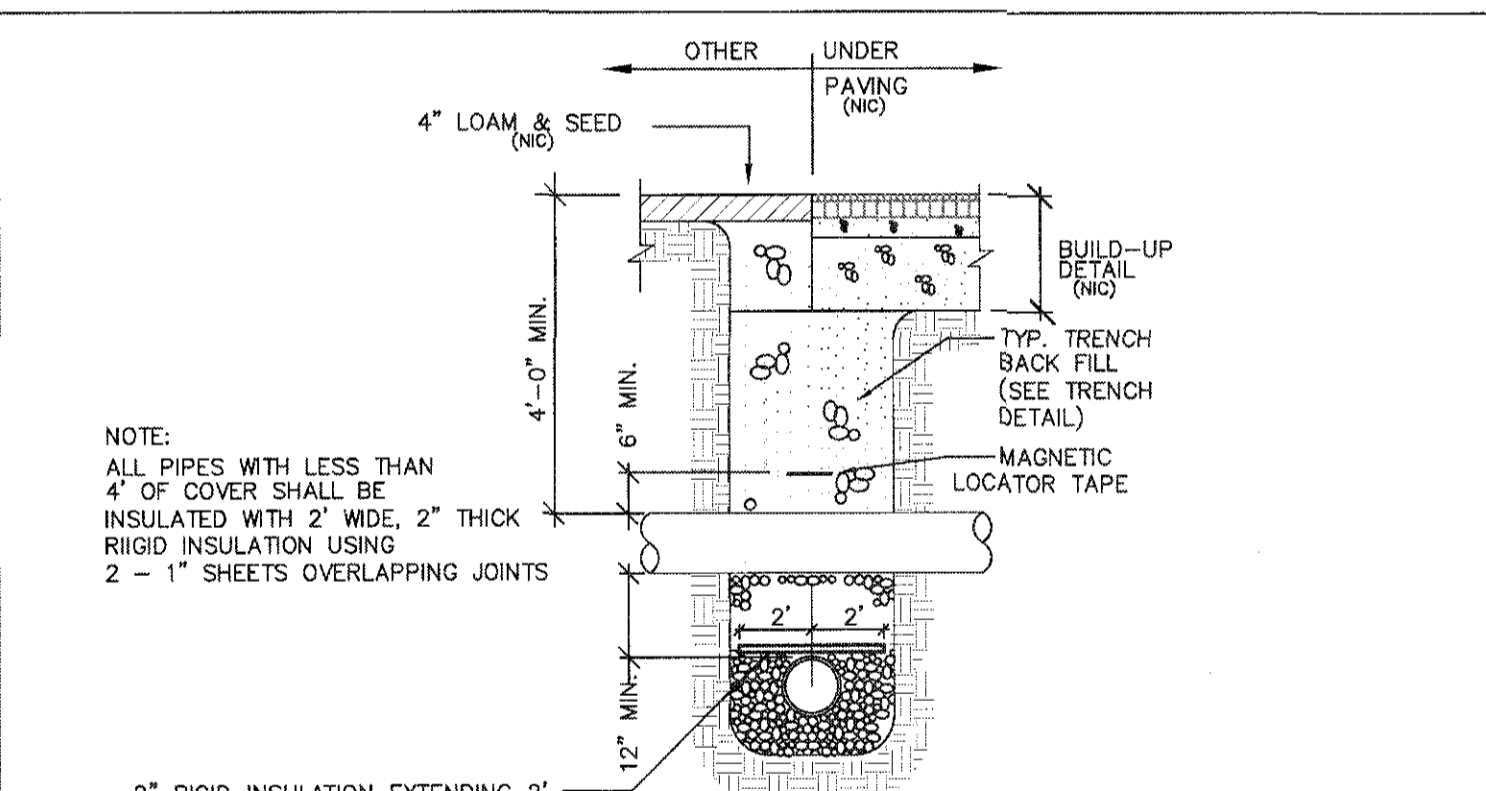
OFF ROAD ELECTRICAL TRENCH DETAIL

NO SCALE
NOTES:
NUMBER OF CONDUIT, MATERIAL, SIZE, & SEPARATION TO BE VERIFIED AT UTILITY PRECONSTRUCTION MEETING.



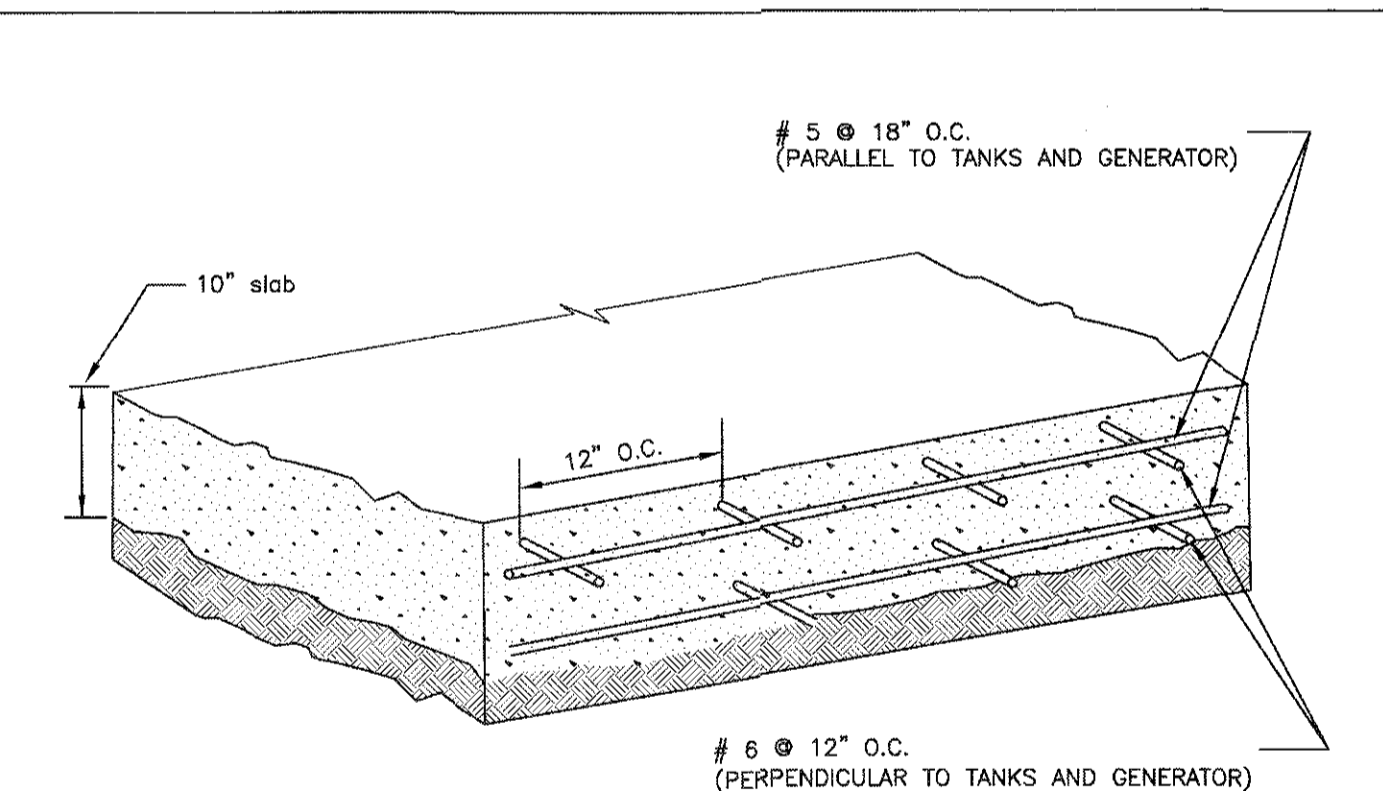
NOTES:
1. COMPACT GRAVEL SUBBASE, BASE COURSE TO 92% OF MAXIMUM DENSITY USING HEAVY ROLLER COMPACTION.
2. CONTRACTOR SHALL SET GRADE STAKES MARKING SUBBASE AND FINISH GRADE ELEVATIONS FOR CONSTRUCTION REFERENCE.

TOWN OFFICE PARKING PAVEMENT BID ALT. #3
NOT TO SCALE



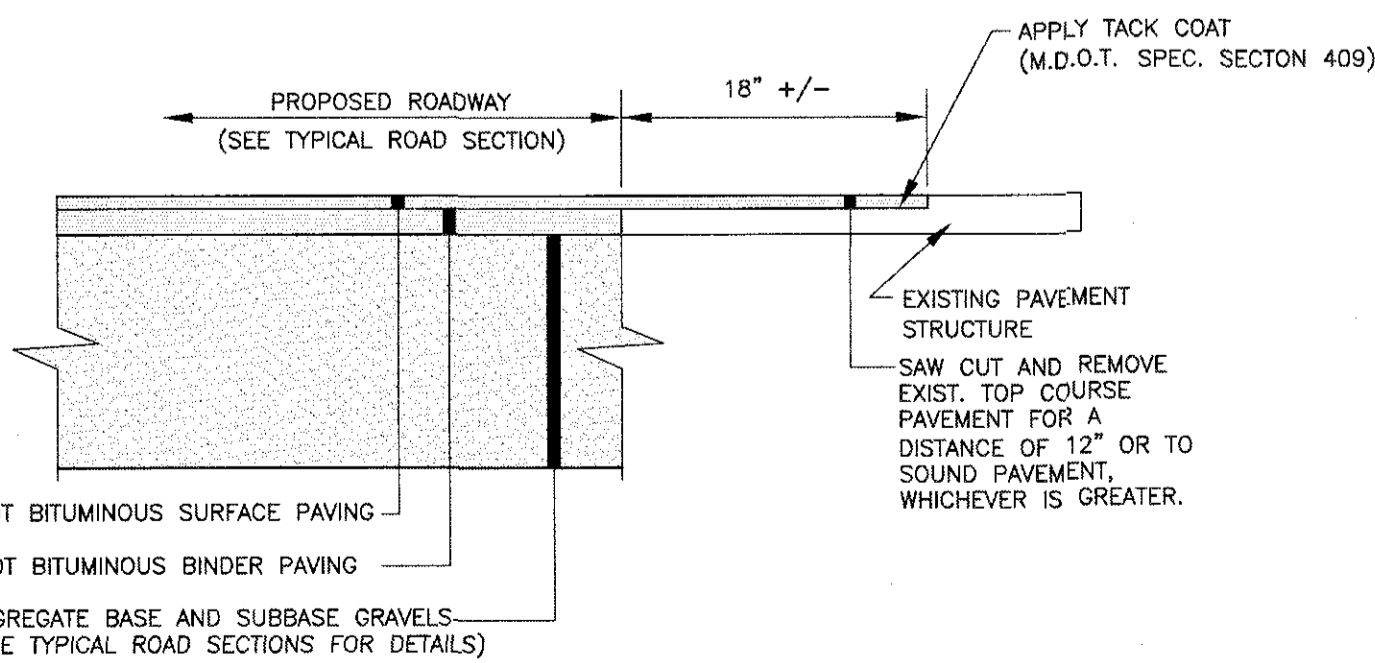
PIPE CROSSING DETAIL

NOT TO SCALE



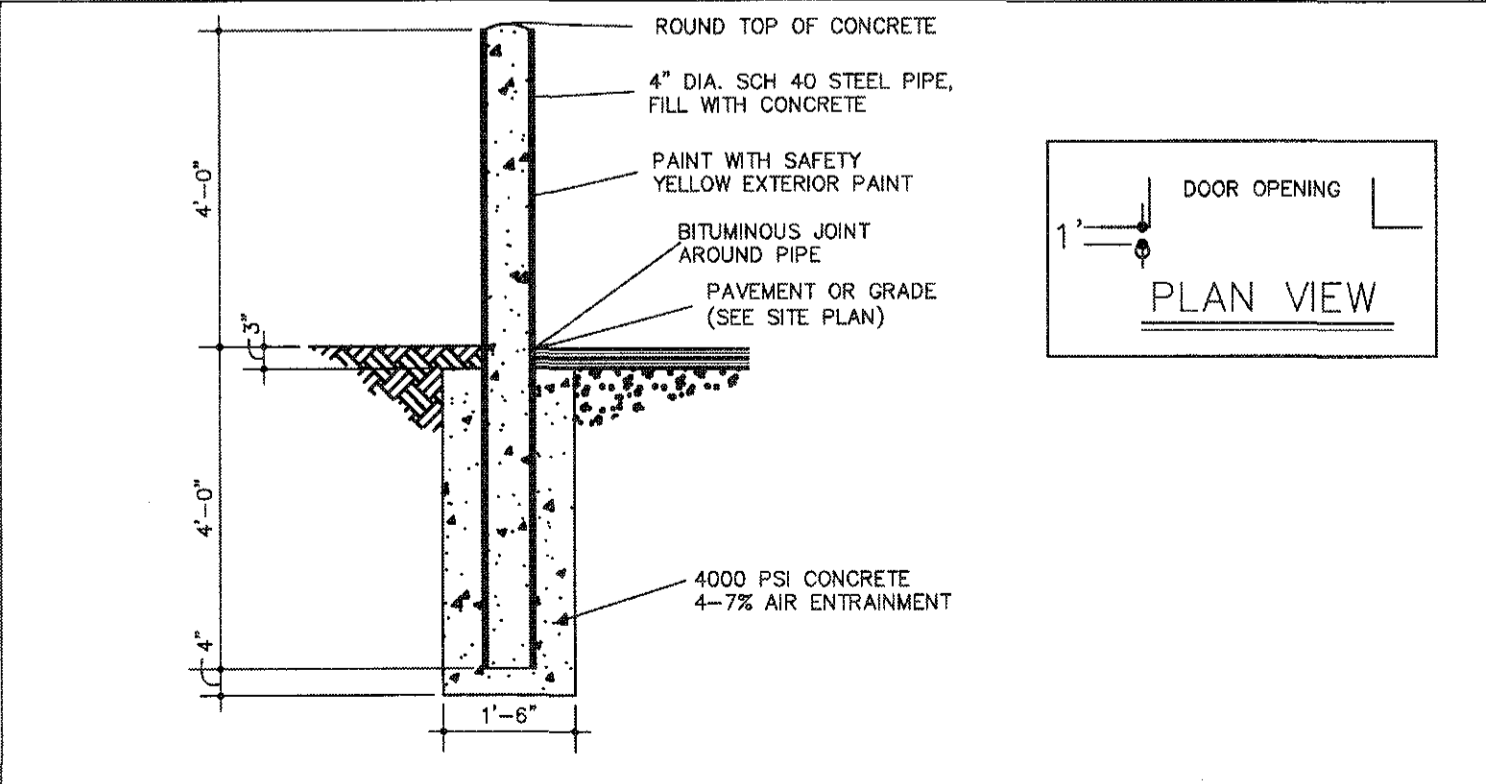
LPG TANK PAD DETAIL

NOT TO SCALE



TYPICAL PAVEMENT JOINT

NOT TO SCALE



CONCRETE BOLLARD DETAIL

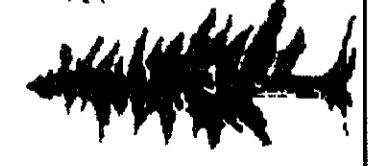
NOT TO SCALE
NOTE:
1. BOLLARDS TO BE PLACED EACH SIDE OF OVERHEAD DOOR, ONE FT. FROM BLDG AND EVEN WITH DOOR OPENING.

NO.	DATE	REVISIONS

PROJECT NAME: NEW FIRE STATION
PROJECT NO.: 1908D
DRAWING NO.:
FIELDBOOK:
SCALE:
DATE ISSUED: 11/21/2023
CLIENT & OWNER: TOWN OF SWEDEN, SWEDEN, ME 04040
DESIGNED BY: J.H.
DRAWN BY: SEB
CHECKED BY: SEB
APPROVED BY: SEB
PLAN DATE: 11/21/2023
CLIENT & OWNER: TOWN OF SWEDEN, SWEDEN, ME 04040

CONSTRUCTION DETAILS
147 BRIDGTON ROAD, SWEDEN, OXFORD CTY MAINE, 04040

Plymouth Engineering, Inc.
8 Main Street, Ste C
Newport, Maine 04903
Tel: (207) 257-2071 Fax: (207) 257-2190
info@plymouthengineering.com
www.plymouthengineering.com



STATE OF MAINE
SCOTT
BRADY
No. 622
11/21/23
REGISTERED PROFESSIONAL ENGINEER

SHEET 5 OF 16

SW-FSBC Exhibit F-6 rev 11-22-2023

C4

Nov 21, 2023 - 2:28pm J:\2019\1908D - Sweden Fire Station - Town of Sweden\Drawings\CIVIL\112123 1908D BASE.dwg



Julia McQueen <jullemcq6@gmail.com>

Sweden Fire Station

1 message

ztilduc@gmail.com <ztilduc@gmail.com>
To: Julia McQueen <jullemcq6@gmail.com>

Mon, Sep 11, 2023 at 9:04 PM

Julia

Planning Boards, as well as DEP, have the prerogative to request a HISS (High Intensity Soil Survey) in lieu of the readily available USDA MISS (Medium Intensity Soil Survey) when they believe the soils on the site are not representative of the MISS for the intended use. Examples of when HISS is desired are dense subdivisions with septic systems, projects with stormwater infiltration systems, land with wetlands where developable area is marginal and lands with high seasonal water tables.

The Skerry soils (HSG "B") represented in the MISS from the USDA Web Soil Survey (WSS) are adequate and sufficient for the intended development. The Site Evaluator has already provided a site soil investigation for the septic system in the only area of the proposed development where definitive soils identification is necessary. As a project reviewer for municipalities, I am more than comfortable with the degree of investigation provided by the USDA WSS as it pertains to this small site expansion.

Regards

Ross Cudlitz, PE

OSWCD District Engineer

Ross A Cudlitz, PE (ME, NH, VT)

CPESC, CPSWQ, CESSWI, CPMSM, 3PI

Engineering Assistance & Design, LLC

10 North Road

Yarmouth, Maine 04096

Cell: (207) 838-7663

ztilduc@gmail.com

Search

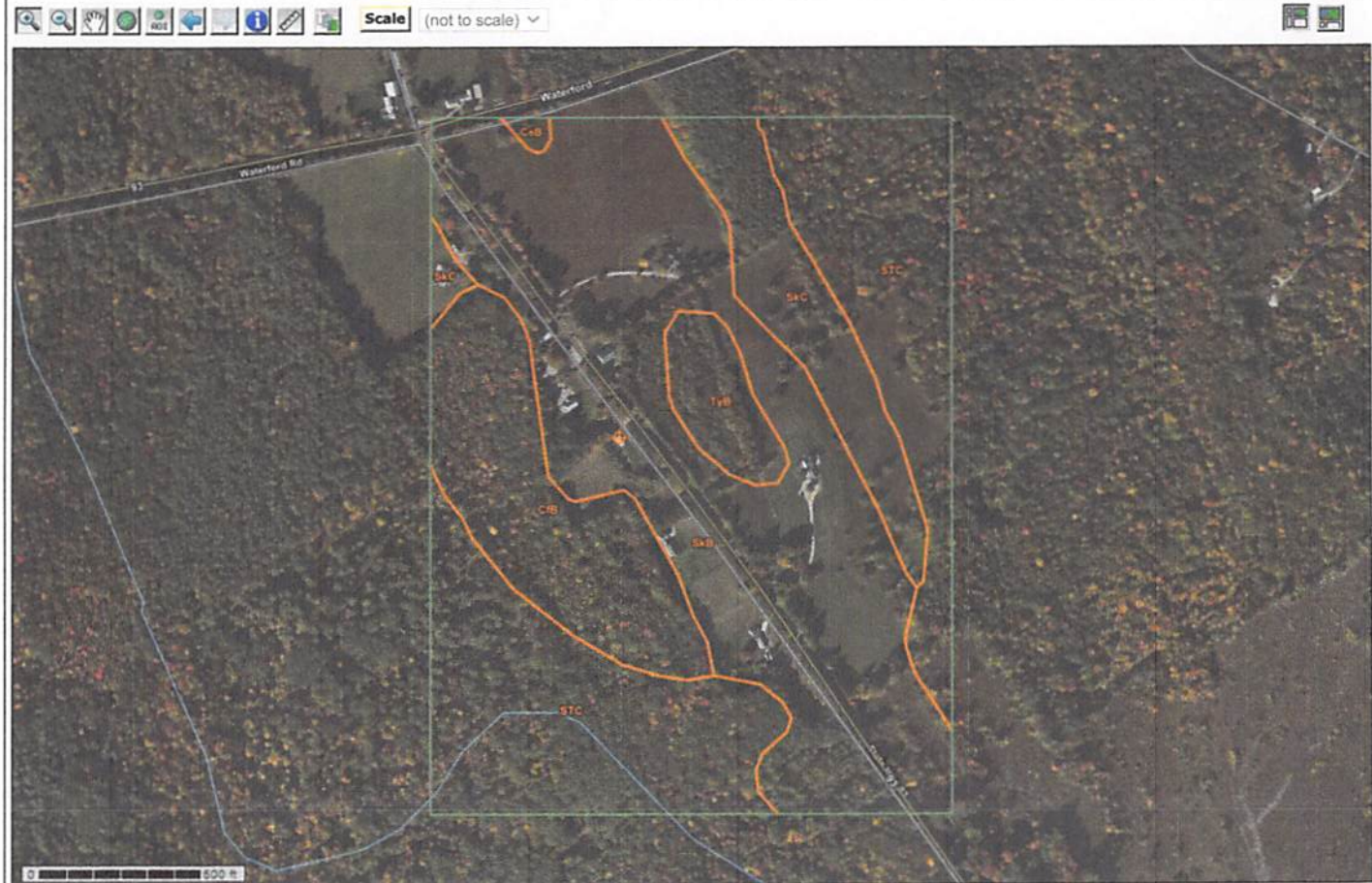
Map Unit Legend

Oxford County Area, Maine (ME613)

Oxford County Area, Maine (ME613)

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
CeB	Colonel fine sandy loam, 3 to 8 percent slopes	0.4	0.3%
CfB	Colonel fine sandy loam, 0 to 8 percent slopes, very stony	15.3	13.4%
SkB	Skerry fine sandy loam, 3 to 8 percent slopes	48.2	42.3%
SkC	Skerry fine sandy loam, 8 to 15 percent slopes	10.3	9.0%
STC	Skerry-Colonel association, 0 to 15 percent slopes, very stony	36.0	31.5%

Soil Map



Warning: Soil Map may not be valid at this scale.

You have zoomed in beyond the scale at which the soil map for this area is intended to be used. Mapping of soils is done at a particular scale. The soil surveys that comprise your AOI were mapped at 1:20,000. The design of map units and the level of detail shown in the resulting soil map are dependent on that map scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.



January 6, 2022

Fire Station Building Committee
Town of Sweden
147 Bridgton Road
Sweden, ME 04040

Re: Soils Suitability and Assessment for Potential Wastewater Disposal, Proposed Fire Barn

Dear Members,

I completed a preliminary soil evaluation at the above-referenced property on December 8, 2021. The purpose of my investigation was to determine the suitability of a portion of the lot (downslope of a conceptual fire barn) for on-site subsurface wastewater disposal.

Enclosed, for your review and use, is a soil test pit profile description. The soils are suitable, as defined by the State of Maine *Subsurface Wastewater Disposal Rules*, in the vicinity of Test Pit A. The soils are classified as 3 C soils per the *Rules*. The soils at the test pit require that a *medium-large* sized (3.3 gal/sq. ft.) disposal field be utilized. The *Rules* state that 20 gallons per day (gpd) per employee present during a 24 hour timespan be utilized in calculating wastewater design flows for the proposed facility. A facility that can accommodate up to 10 employees per day (200 gpd) would require a 20' x 35' stone & pipe disposal field (or equivalent) and a 1,000 gallon septic tank. The site identified will require fill for construction above pre-existing grades. Building placement and grading may allow for a gravity flow system.

Also included is my *Soil Test Pit and Potential Wastewater Disposal Plan*. This plan depicts the location of the test pit excavated, your potential fire barn location (as indicated on the plan entitled *Fire Station Location Sketch* dated February 21, 2019), the approximate existing wastewater disposal field (per the permitted HHE-200 Form by Walter Horton, dated September 24, 2003), and other features relevant to a future septic system design (abutter's well, existing structures, etc.). My potential wastewater disposal field, as shown, is sized for up to 10 employees. Potential disposal field configuration and sizing as shown is to accommodate only the wastewater generated by the employees working at the proposed fire barn. Addition of a "commercial kitchen" will require investigations over of a much larger area of the site to assure soils suitability and conformance to setbacks.

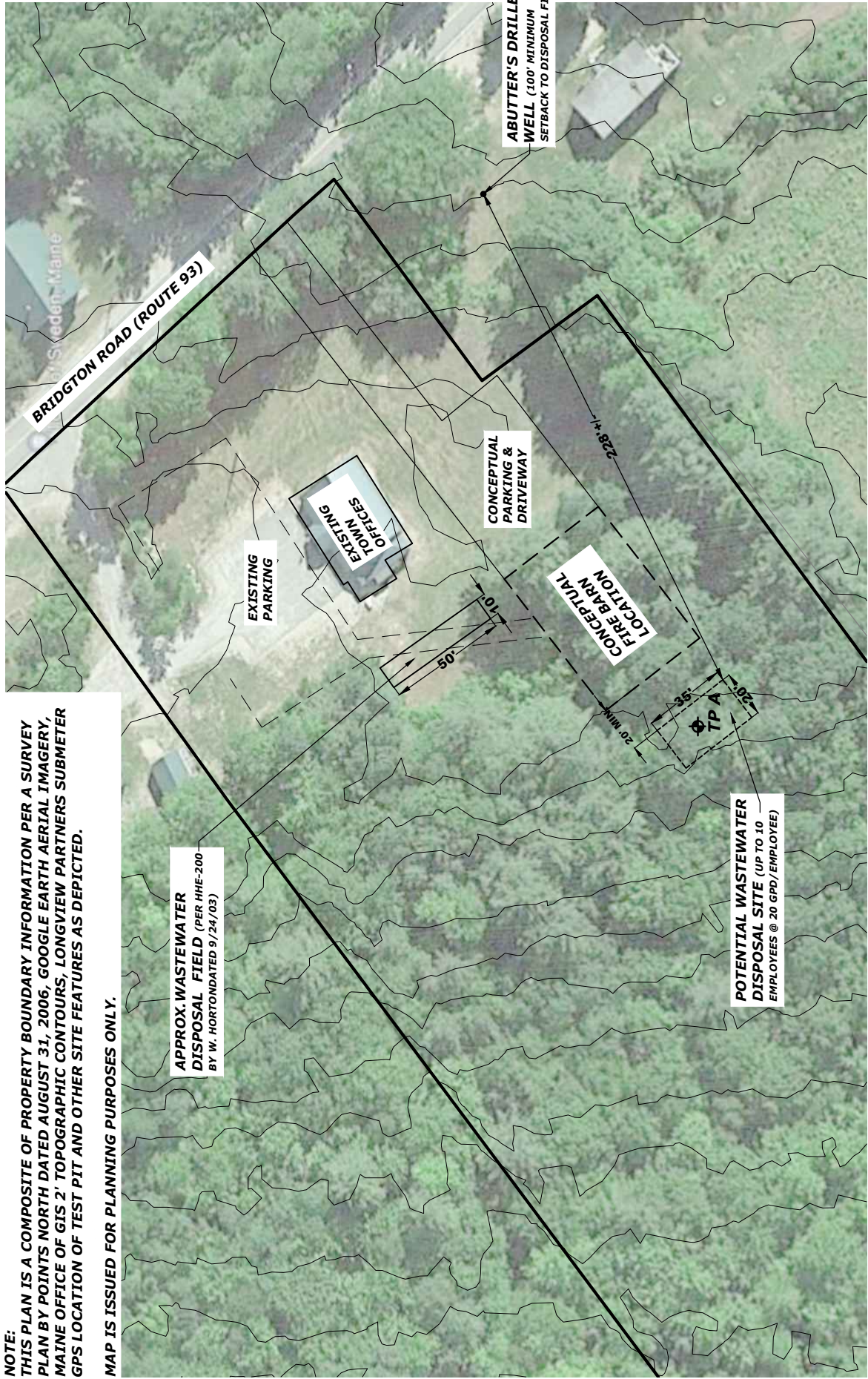
A completed subsurface wastewater disposal system design (HHE-200 form) is required to obtain permits to install a system once building location, building size, and overall site development are finalized.

Should you have any questions regarding the results of my work, please feel free to call.

Sincerely,

William H. O'Connor
Longview Partners, LLC
Licensed Site Evaluator # 363
Professional Wetland Scientist

NOTE:
 THIS PLAN IS A COMPOSITE OF PROPERTY BOUNDARY INFORMATION PER A SURVEY PLAN BY POINTS NORTH DATED AUGUST 31, 2006, GOOGLE EARTH AERIAL IMAGERY, MAINE OFFICE OF GIS 2' TOPOGRAPHIC CONTOURS, LONGVIEW PARTNERS SUBMETER GPS LOCATION OF TEST PIT AND OTHER SITE FEATURES AS DEPICTED.
 MAP IS ISSUED FOR PLANNING PURPOSES ONLY.

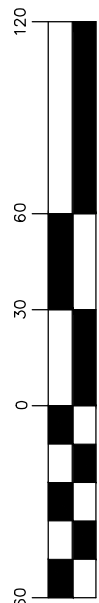


APPROX. WASTEWATER DISPOSAL FIELD (PER HHE-200 BY W. HORTON DATED 9/24/03)

POTENTIAL WASTEWATER DISPOSAL SITE (UP TO 10 EMPLOYEES @ 20 GPD/EMPLOYEE)

ABUTTER'S DRILLED WELL (100' MINIMUM SETBACK TO DISPOSAL FIELD)

GRAPHIC SCALE



(IN FEET)
 1 inch = 60 ft.

SOIL TEST PIT & POTENTIAL WASTEWATER DISPOSAL PLAN
 PREPARED FOR
SWEDEN FIRE STATION BUILDING COMMITTEE
 147 BRIDGTON ROAD
 (MAP R-5, LOT 43A)
 SWEDEN, MAINE



ENVIRONMENTAL PERMITTING SPECIALISTS

DRAFT: BO
 SCALE: 1" = 60'
 CHECKED: BO
 PLAN DATE: 1/6/22



Maine Department of Transportation

Janet T. Mills
Governor

Driveway/Entrance Permit

Bruce A. Van Note
Commissioner

Permit Number: 33900 - Entrance ID: 1

LOCATION

OWNER
Name: **Town of Sweden**
Address: **147 Bridgton Road**
Sweden, ME 04009
Telephone: **(207)647-3944**

Route: **0093X, Bridgton Road**
Municipality: **Sweden**
County: **Oxford**
Tax Map: **R05 Lot Number: 43A**
Culvert Size: **15 inches**
Culvert Type: **plastic**
Culvert Length: **55 feet**
Date of Permit: **November 17, 2022**
Approved Entrance Width: **30 feet**

Date Printed: **November 17, 2022**

In accordance with rules promulgated under 23 M.R.S.A., Chapter 13, Subchapter I, Section 704, the Maine Department of Transportation (MaineDOT) approves a permit and grants permission to perform the necessary grading to construct, in accordance with sketch or attached plan, **an Entrance to Fire Station** at a point **1163 feet South from Waterford Road**, subject to the Chapter 299 Highway Driveway and Entrance Rules, standard conditions and special conditions (if any) listed below.

Conditions of Approval:

This Permittee acknowledges and agrees to comply with the Standard Conditions and Approval attached hereto and to any Specific Conditions of Approval shown here.

(G = GPS Location; W = Waiver; S = Special Condition)

G - THE ENTRANCE SHALL BE LOCATED AT GPS COORDINATES: 44.132948N, -70.806446W.

S - In the town of Sweden on the southwesterly side of Route 93, the centerline being approximately 1163 feet southeasterly of the centerline of Waterford Street and approximately 77 feet southeasterly of utility pole 244.

S - The culvert shall be HDPE smoothbore plastic pipe. The property owner must contact MaineDOT at (207) 935-2646 prior to culvert and entrance installation to review procedures and arrange an inspection.

Approved by:  Date: 11-17-2022

SW-FSBC CUP Appl. Exhibit I-1

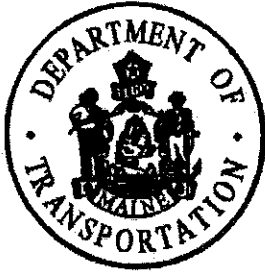
STANDARD CONDITIONS AND APPROVAL

1. Provide, erect and maintain all necessary barricades, lights, warning signs and other devices as directed by MaineDOT to properly safeguard traffic while the construction is in progress.
2. At no time cause the highway to be closed to traffic
3. Where the driveway is located within a curb, curb and gutter, and/or sidewalk section, completely remove the existing curb, curb and gutter, and/or sidewalk as may be required to create the driveway and restore drainage. All driveways abutting sidewalk sections shall meet the requirements set forth in the Americans with Disabilities Act of 1990, 42 U.S.C. Sec. 12131 et seq.
4. Obtain, have delivered to the site, and install any culverts and/or drainage structures which may be necessary for drainage, the size, type and length as called for in the permit pursuant to 23 M.R.S.A. Sec. 705. All culverts and/or drainage structures shall be new.
5. Start construction of the proposed driveway within twenty-four (24) months of the date of permit issuance and substantially complete construction of the proposed driveway within twelve months of commencement of construction.
6. Comply with all applicable federal, state and municipal regulations and ordinances.
7. Do not alter, without the express written consent of the MaineDOT, any culverts or drainage swales within the MaineDOT right of way.
8. File a copy of the approved driveway permit with the affected municipality or LURC, as appropriate within 5 business days of receiving the MaineDOT approval.
9. Construct and maintain the driveway side slopes to be no steeper than the adjacent roadway side slopes, but in no case to be steeper than 3 horizontal to 1 vertical, unless the side slope is behind existing roadway guardrail, in which case it shall be no steeper than 2 horizontal to 1 vertical.
10. Notify the MaineDOT of a proposed change of use served by the driveway when increase in traffic flow is expected to occur. This does not exempt the need for obtaining a Traffic Movement Permit (TMP) if trip generation meets or exceeds 100 passenger car equivalents (PCE) during the peak hour of the day.
11. Construct or implement and maintain erosion and sedimentation measures sufficient to protect MaineDOT facilities.
12. Driveways shall be designed such that all maneuvering and parking of any vehicles will take place outside the highway right-of-way and where vehicles will exit the premises without backing onto the highway traveled way or shoulders. All driveways will have a turnaround area to accommodate vehicles using the premises.
13. Closing any portion of a highway or roadway including lanes, shoulders, sidewalks, bike lanes, or ATV access routes is not permitted without MaineDOT approval.

FURTHER CONDITION OF THE PERMIT

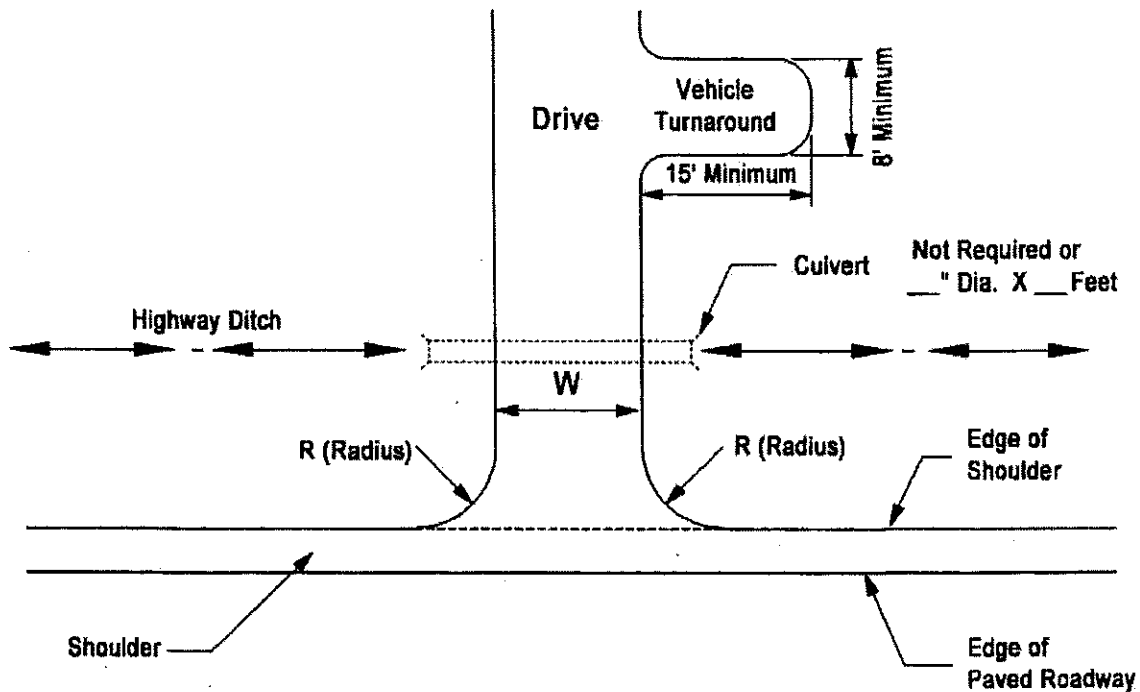
The owner shall assume, the defense of, and pay all damages, fines, and penalties for which he/she shall become liable, and shall indemnify and safe harmless said Department, its representatives, agents and employees from liability, actions against all suits, claims, damages for wrongful death, personal injuries or property damage suffered by any person or association which results from the willful or negligent action or inaction of the owner/applicant (agent) and in proceedings of every kind arising out of the construction and maintenance of said entrance(s), including snow removal.

Nothing herein shall, nor is intended to, waive any defense, immunity or limitation of liability which may be available to the MaineDOT, their officers, agents or employees under the Maine Tort Claims Act or any other privileges and/or immunities provided by law. It is a further condition that the owner will agree to keep the right of way inviolate for public highway purposes and no signs (other than traffic signs and signals), posters, billboards, roadside stands, culvert end walls or private installations shall be permitted within Right of Way limits.



State of Maine
Department of Transportation
Entrance / Driveway Details

PLAN

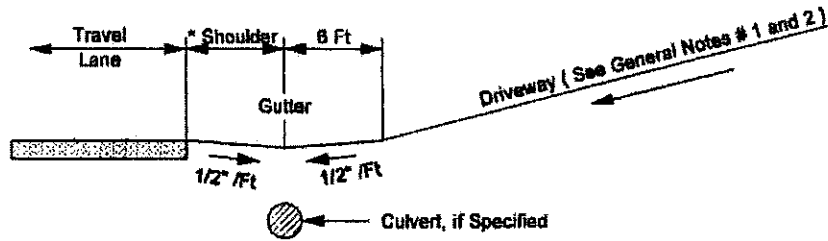


GENERAL NOTES -

1. ALL RESIDENTAL OR COMMERCIAL DRIVES WITH 10% GRADE OR MORE SLOPING DOWN TOWARDS THE HIGHWAY SHALL BE PAVED TO THE RIGHT OF WAY LINE, AS A MINIMUM, INCLUDING SHOULDER, IF GRAVEL AND HAVE DITCHES TO CONTROL RUNOFF.
2. DRIVES SLOPING TO THE HIGHWAY SHALL BE CROWNED (1/2" PER FT. MINIMUM).
3. TO THE MAXIMUM EXTENT PRACTICAL, THE ENTRANCE MUST BE CONSTRUCTED PERPENDICULAR TO THE HIGHWAY AT THE POINT OF ACCESS. EXCEPT WHERE CURBING EXISTS OR IS PROPOSED, THE MINIMUM RADIUS ON THE EDGES OF THE ENTRANCE MUST BE 10 FEET OR AS OTHERWISE REQUIRED AS SHOWN.
4. ENTRANCES/DRIVEWAYS WILL BE BUILT WITH AN ADEQUATE TURN-AROUND AREA ON SITE TO ALLOW ALL VEHICLES TO MANUEVER AND PARK WITHOUT BACKING ONTO THE HIGHWAY. THIS TURN-AROUND SHALL BE AT LEAST 8 FEET WIDE BY 15 FEET LONG.
5. ENTRANCES/DRIVEWAYS AND OTHER ASSOCIATED SITE WORK WHICH DIRECTS WATER (RUNOFF) TOWARD THE HIGHWAY MUST BE CONSTRUCTED, CROWNED STABILIZED AND MAINTAINED WITH MATERIALS AND APPROPRIATE TEMPORARY/PERMANENT EROSION CONTROL MATERIALS IN ACCORDANCE WITH MDOT BEST MANAGEMENT PRACTICES.
6. THE PROFILE OF THE ENTRANCES MUST COMPLY WITH THE DETAILS SHOWN ON PAGE 2.

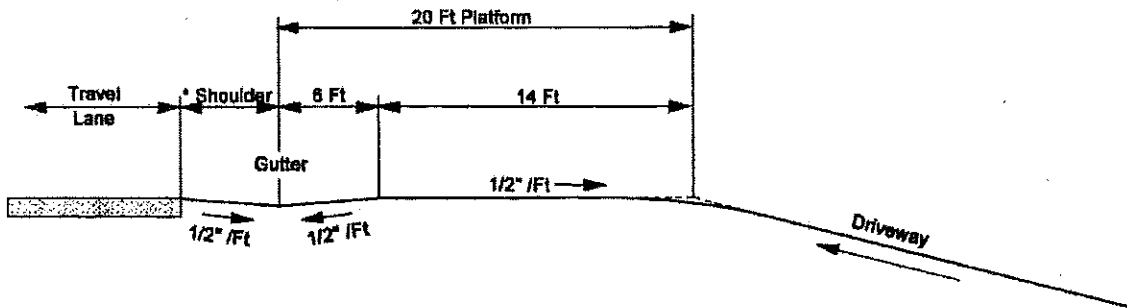
MDOT Entrance / Driveway Details, Continued

PROFILE Details

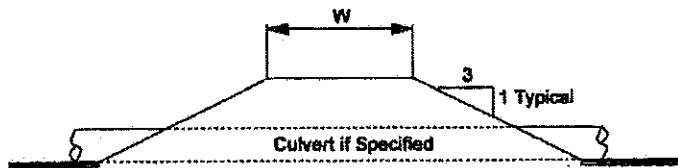


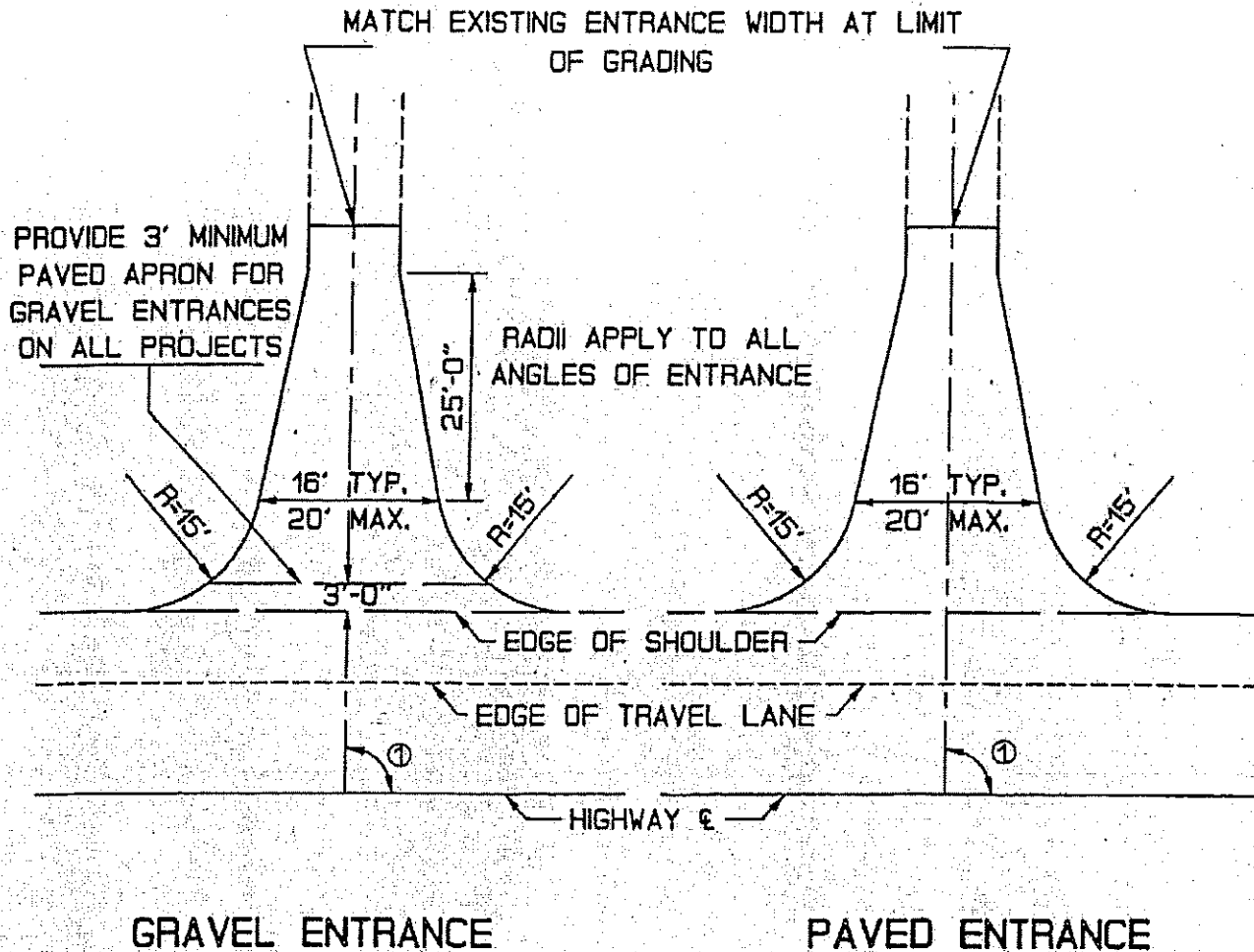
NOTE :

Grade of Existing Shoulder Should Be Maintained To Create A Gutter With a Minimum Of Three Inches Below The Edge Of Traveled Way.
 \ast Distance Of The Gutter From The Edge Of Traveled Way Should Be The Same As Existing Shoulder Or A Minimum Of 4 Feet.



Driveway Cross Section



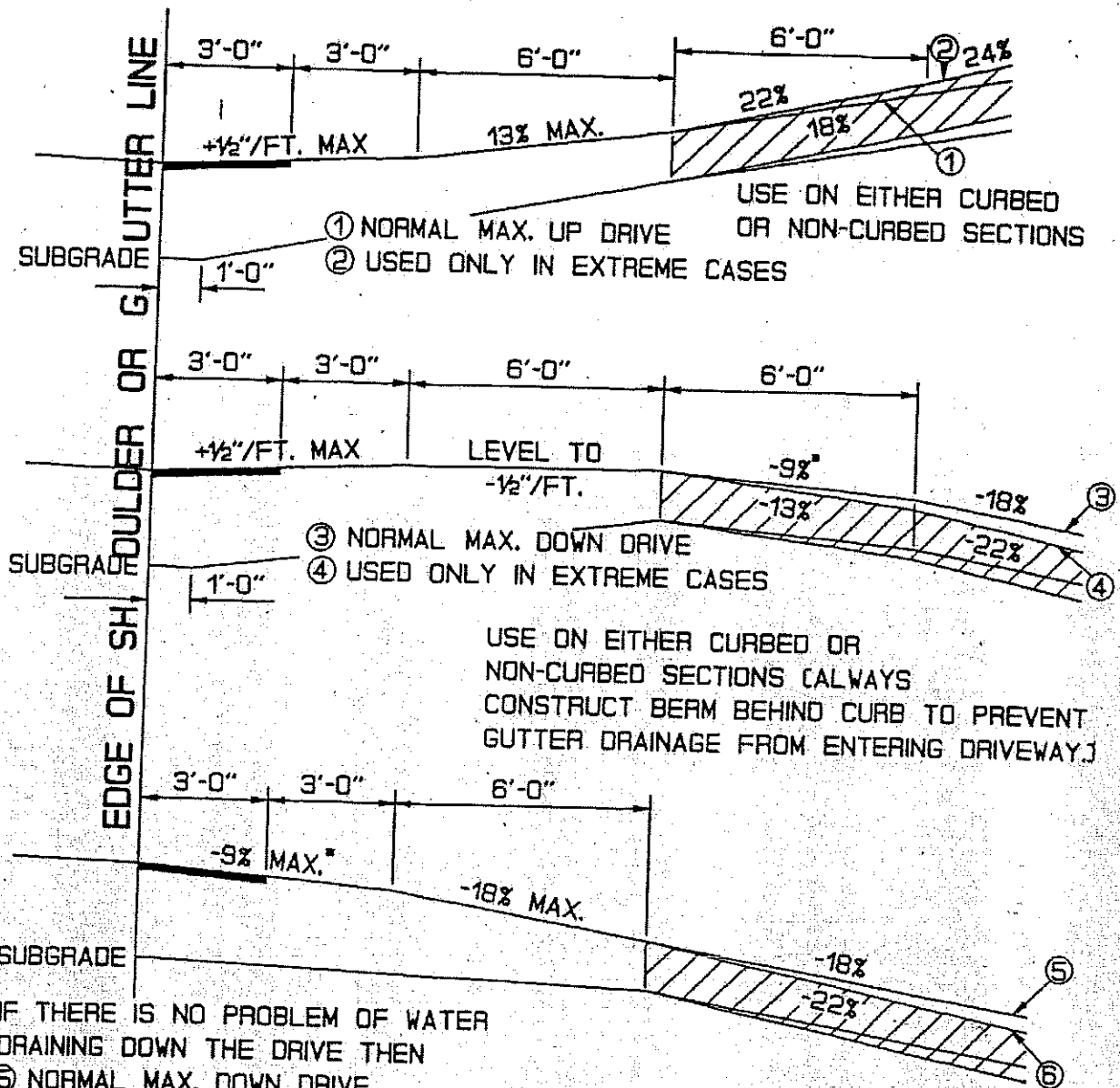


⓪ ENTRANCE ANGLE SHOULD NOT BE LESS THAN 45°

**RESIDENTIAL ENTRANCE
ONTO UNCURBED HIGHWAY
(PAVED SHOULDERS)**

SW-FSBC CUP Appl. Exhibit I-5

EN003



① NORMAL MAX. UP DRIVE
 ② USED ONLY IN EXTREME CASES

USE ON EITHER CURBED OR NON-CURBED SECTIONS

③ NORMAL MAX. DOWN DRIVE
 ④ USED ONLY IN EXTREME CASES

USE ON EITHER CURBED OR NON-CURBED SECTIONS [ALWAYS CONSTRUCT BERM BEHIND CURB TO PREVENT GUTTER DRAINAGE FROM ENTERING DRIVEWAY.]

⑤ NORMAL MAX. DOWN DRIVE
 ⑥ USED ONLY IN EXTREME CASES

IF THERE IS NO PROBLEM OF WATER DRAINING DOWN THE DRIVE THEN

USE ONLY ON NON-CURBED SECTIONS ON SEASONAL OR LIMITED USE ENTRANCES OR WHEN DRAINAGE IS NOT A FACTOR.

GENERAL NOTES:

1. THE FIRST 3' SHOWN AS PAVEMENT SHALL BE PAVED ONLY WHEN ABUTTING A PAVED AREA.
2. ALL RESIDENTIAL OR COMMERCIAL DRIVES 10% AND OVER SHALL BE PAVED.

NOTES ON MAXIMUM DRIVEWAY PROFILES:

1. THESE PROFILES ARE A GUIDE FOR THE MAJORITY OF CASES, BUT SHOULD BE FIELD CHECKED WHEN THE MAIN LINE GRADE IS STEEP (4% TO 6% OR GREATER) OR THE ANGLE OF APPROACH TO THE DRIVE IS UNUSUAL.
2. GENERALLY THE MAJORITY OF DRIVES ON A PROJECT WILL BE BUILT WITH FLATTER PROFILES THAN THESE MAXIMUM CASES.
3. WHEN GRADING DRIVES WHICH ARE FLATTER THAN THE MAXIMUM PROFILES THE FOLLOWING RULE OF THUMB SHOULD BE USED: DO NOT EXCEED A GRADE % CHANGE OF MORE THAN 9% IN A 6 FOOT INCREMENT OF DRIVEWAY LENGTH. THIS APPLIES TO BOTH UP AND DOWN PROFILES.

DRIVES ON NON-SIDEWALK SECTIONS EN002

**Town of Sweden Fire Station Building Committee (FSBC) Meeting Record
Monday October 2, 2023, 2 pm at the Sweden Town Meeting House**

Pledge of Allegiance

1. **Call to Order** at 2:04 pm at the Sweden Town Meeting House

2. **Introduction of Attendees:** FSBC Members Frank Howell and Julie McQueen, Plymouth Engineering Engineer Keith Ewing, Select Board (SB) Member Dan McQueen, Fire Chief Tim Cook, Assistant Fire Chief Tommie McKenzie, Resident Taxpayer George Conly

3. **Communication and Bills:**

4. **Old Business:**
 - A. Review and Revise: Project Manual for Sweden Fire Station dated October 2022:
 - It will take Plymouth at least 3 weeks to do the paperwork to get the bid documents ready for the SB to approve. They should be ready by Monday October 23rd. The SB has a regularly scheduled meeting on Tuesday October 24th. If they approve it and give Plymouth the directive to move forward the project could be advertised for bid around October 31st.
 - Tommie McKenzie (EMA Director) believes that the Town will know by the end of October or the middle of November whether or not the Congressionally Directed Spending Funds (requested \$1,150,000) will be received by the Town.
 - Keith's best engineer's estimate for the project is \$300/SF based on bids received for a similar project last week. The building is 4,864 SF X \$300 = \$1,459,200 + contingencies – Keith's estimate is about \$1,780,000. Will also need some additional \$ for review and approval by Plymouth Engineering of the mechanical and electrical plans and specs, the Conditional Use Permit, Building Permit, Bond Counsel, and third-party inspections (Plymouth will give us a price to assist with this) and utilities during construction.
 - Per Tommie McKenzie the Town has abandoned the pursuit of a USDA loan.
 - Will likely go through Norway Savings Bank (the bank the Town uses currently) or the Maine Bond Bank if the Congressionally Directed Spending funds are not approved.
 - Specs for mechanical and electrical work are design/build which could be a problem if the Congressionally Directed Spending Funds are received, depending on what agency oversees the project. Federal \$ has more requirements than a commercial bank loan.
 - Need to address what changes to the contract will be required if the Town does receive Congressionally Directed Spending Funds and how to implement them.
 - The group agreed to have the Town pay for heat and electricity during construction because per Keith – in his experience the Town will pay 5% more if the contractor has to include it in their price.
 - Keith will look at the power source after the meeting to determine if the contractor will need temporary power or can just use the Town office power supply.
 - Plymouth will conduct a pre-bid meeting and site walk at the Town office on a Mon., Wed. or Friday (since the Town office is not open for business on those days). Will take and provide meeting minutes for the meeting.
 - Questions about heat for the station: The current trend for fire stations is radiant in the slabs of the equipment bays, with forced hot air Modine heaters to help the area recover if the doors are open.
 - Plymouth does not recommend oil fired radiant in the office spaces – that will be forced hot air with an Energy Recovery Ventilator (ERV) air exchange unit is proposed in those areas and in the gear decontamination room.
 - **Plymouth, the Fire Chief and the Assistant Fire Chief all confirmed that the station would not generate any hazardous materials.**
 - The Town purchased a generator for the existing station which is large enough to power the new proposed station.
 - The committee consensus is that the station should be fueled by propane instead of oil after recommendations from Plymouth and Frank.
 - Will need funds for Construction Administration:
 - Mechanical and Electrical package review

**Town of Sweden Fire Station Building Committee (FSBC) Meeting Record
Monday October 2, 2023, 2 pm at the Sweden Town Meeting House**

- Rebar Submittal review
- Shop drawing and mechanical system review.
- Plymouth will price one site visit per month to be sure that the project meets the requirements.
- Will also provide an hourly rate for phone calls – so Plymouth and the Clerk if the Works (probably Frank) can talk monthly to address pay requisitions and be sure that work completed is satisfactory and the contractor should be paid.
- Will also need a Finance Manager (will this be the SB?) to make sure that funding is available to cover the pay requisitions. Budget Committee could assist.
- Tim Cook wanted to know what the concrete specifications called for. Per Keith, the slab is 4,000 psi and the walls are 3,000 psi no fiber mesh or welded wire. He specified rebar partly because of the radiant heat.
- **Staton will provide prep for a super duty extraction machine so what goes into the septic system will be clean. Residual material from an oil water separator should not go into the septic system.**

5. New Business:

A. Address bid schedule

- Keith Ewing walked through Plymouth Engineering's potential Timeline for bidding and awarding the contract:
- 10-23-2023: (Monday) Finalize the bid package.
- 10-24-2023: (Tuesday) SB review and give Plymouth Engineering directive to proceed.
- 10-31-2023 (Tuesday) Request for Proposals (RFP)
Allow 5-6 weeks until bid opening.
- Per Tommie McKenzie – should know about Congressionally Directed Spending Grant by the end of October or beginning of November 2023.
- 12-4-2023 to 12-13-2023: (Monday or Wednesday): Bid Opening
- 1-6-2024 (Saturday) Special Town Meeting (STM) for voter approval (weather date of 1-13-2024)
Need to post meeting at least 7 days prior to the meeting.
If going through the Maine Bond Bank for financing:
- 2-1-2024: (Thursday) Maine Bond Bank Application Due (This is an approximate date)
- 2-2-2024: (Friday) If bids are good for 60 days from December 4th, be prepared to award the contract by this date.
- 4-15-2024: Pricing Due (this means the Bond Bank will know the rates) (This is an approximate date)
- 5-15-2024: (Wednesday) Funds Available May 15th (This is an approximate date)

B. Address request for additional funding from Plymouth Engineering. After the meeting, Julie, Frank & Keith reviewed contract documents and Plymouth's letter dated 9-20-2023 which included a budget increase request of \$5,100. Currently, we are over the final bid budget by \$3,634.27 Julie will send supporting documents and summary so Plymouth can submit a revised request. The SB will determine the outcome.

D. Address Conditional Use Permit Application: Julie will continue to work on it and get it submitted to the Planning Board.

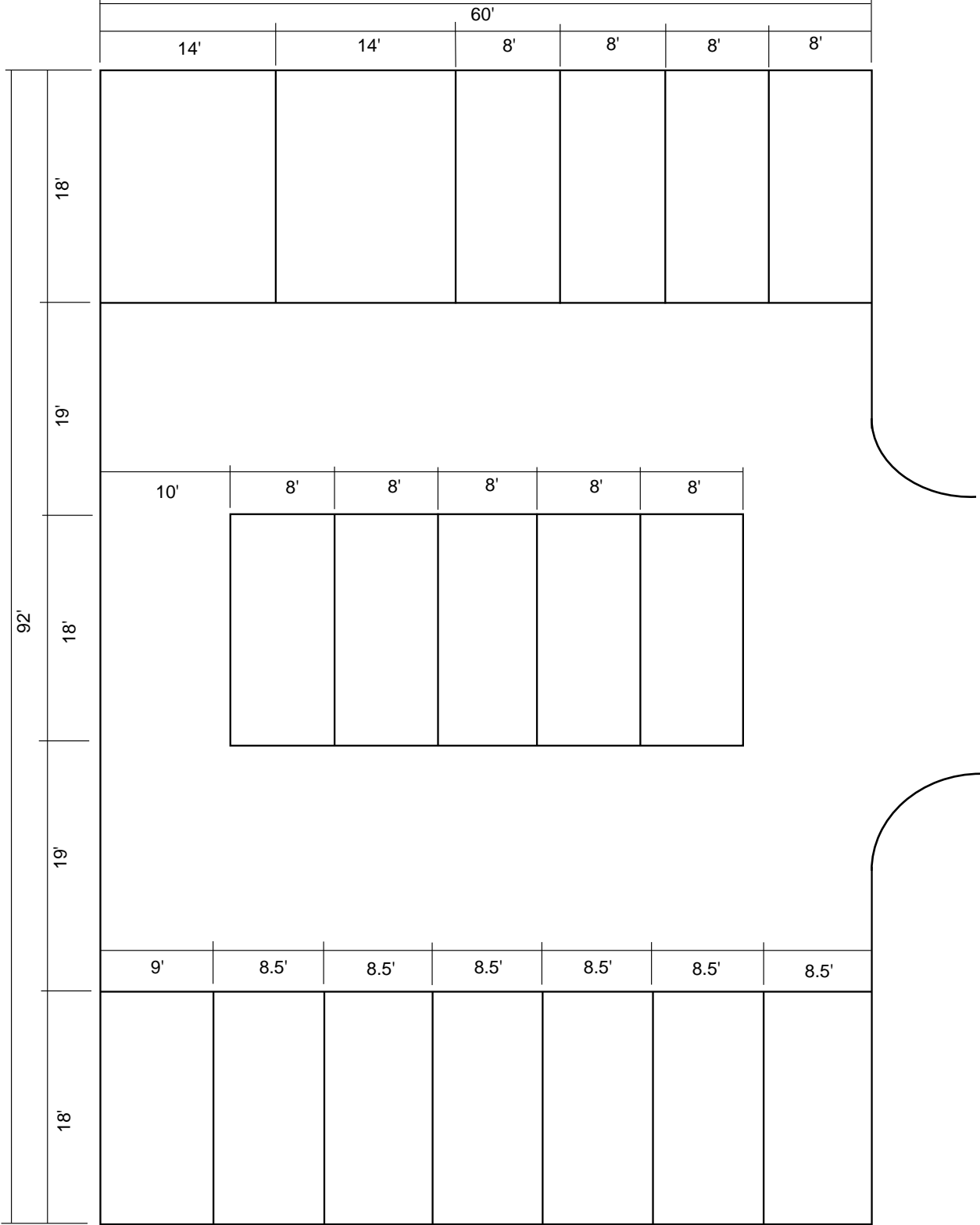
E. Julie will contact Bond Counsel to get some information on wording for the bid documents for Keith.

F. Address letter to be sent to the SB: Will wait to hear back from Keith Ewing on schedule and request for additional funding.

6. Meeting Adjourned at 3:05 pm

Town of Sweden Proposed Parking Lot

Sweden Town Office





STATE OF MAINE - DEPARTMENT OF PUBLIC SAFETY
OFFICE OF STATE FIRE MARSHAL
45 COMMERCE DR STE 1
AUGUSTA, ME 04333-0001

Construction Permit

No. 29159

In accordance with the provisions of M.R.S.A. Title 25, Chapter 317, Sec.317 and Title 5, Section 4594-F, permission is hereby granted to construct or alter the following referenced building according to the plans hitherto filed with the Commissioner and now approved. No departure from application form/plans shall be made without prior approval in writing. Nothing herein shall excuse the holder of this permit for failure to comply with local ordinances, zoning laws, or other pertinent legal restrictions.

Each permit issued shall be displayed at the site of construction.

Building: SWEDEN FIRE STATION
Location: 147 BRIDGTON RD, SWEDEN, ME 04040-5239
Owner: TOWN OF SWEDEN
Owner Address: TOWN OF SWEDEN
147 BRIDGTON RD
SWEDEN, ME 04040-5239

Occupancy Type: Business
Secondary Use: Storage
Use Layout: Separated Use
No Sprinkler System
No Fire Alarm System
Barrier Free
Construction Mode: New Building
Unprotected Wood Frame: Type V (000)
Final Number of Stories: 1

Permit Date: 08/15/2023

Expiration Date: 01/01/2024


COMMISSIONER OF PUBLIC SAFETY

Copy 1 - Owner

SW-FSBC CUP Appl. Exhibit L